

3.0 ASSESSMENT

3.2 PLANNING POLICY & GUIDANCE

National Planning Policy

Relevant Central Government Policy is contained within the National Planning Policy Framework (NPPF) adopted in July 2018, and the Planning Practice Guidance (PPG), launched online in March 2014.

Achieving Sustainable Development

The NPPF is underpinned by the presumption in favour of sustainable development applied by plan making and decision making.

Paragraph 8 identifies sustainable development as having three overarching objectives – economic, social and environmental, which are interdependent and need to be pursued in mutually supporting ways.

The Green Belt

The Government attaches great importance to Green Belts, whose primary purpose is to prevent urban sprawl by keeping land permanently open.

Paragraph 134 sets out the five purposes of Green Belt land:

1. To check the unrestricted sprawl of large built-up areas;
2. To prevent neighbouring towns merging into one another;
3. To assist in safeguarding the countryside from encroachment;
4. To preserve the setting and special character of historic towns, and
5. To assist in urban regeneration, by encouraging the recycling of derelict and other urban land.

Appropriate development in the Green Belt is identified at paragraph 145, including:

- The provision of appropriate facilities (in connection with the existing use of land or a change of use) for outdoor sport and recreation, subject to preserving the openness of the Green Belt, and
- Limited infilling or partial or complete development of previously developed land, which would not have a greater impact on Green Belt openness than the existing development, or not cause substantial harm to the openness of the Green Belt, where development would re-use previously developed land and contribute to towards an identified affordable housing need.

Other forms of development including material changes in the land use for recreation, is also appropriate in the Green Belt provided its openness and purposes are preserved according to paragraph 146.

Achieving Well-Designed Places

Paragraph 124 explains that good design is a key aspect of sustainable development, with effective engagement between applicants, communities and LPAs encouraged.

A number of design criteria applicable to new development are set out in paragraph 127 including function, visual attractiveness, local character (whilst not preventing or discouraging appropriate innovation or change, including increased densities), a sense of place, accessibility and security.

In determining planning applications, paragraph 131 confirms that great weight should be given to outstanding or innovative design which promotes high levels of sustainability, or helps raise the standard of design more generally in the area, so long as it fits with the overall form, layout and surroundings.

Promoting Sustainable Transport

Paragraph 102 confirms that development proposals should address any potential impact on the transport network, and promote opportunities for walking, cycling and public transport in order to contribute to making high quality places.

In terms of car parking provision, paragraph 105 advises that local residential/non-residential parking standards should take into account, inter alia, accessibility, type, mix and use of development, and availability of public transport.

Paragraph 109 states that development should only be prevented or refused on highway grounds when there would be an unacceptable impact on highway safety, or if the residual cumulative impacts on the road network are severe.

4.0 EVALUATION

4.1 SITE INFLUENCES

Constraints

- Located within Metropolitan Green Belt;
- In operation as a racecourse;
- A number of heritage assets;
- Protected trees; and
- Flood zone 2/drainage to the north.

Opportunities

- Delivery of additional development in sustainable locations close to existing services and facilities;
- To improve connectivity between train station and town centre;
- Largely utilise previously developed land;
- Infill and rounding off of existing development clusters;
- Enhancement of existing racing facilities within the developed area of the Masterplan Site;
- Utilise and upgrade existing access; and
- Extend town centre activity and enhance connectivity with the racecourse.

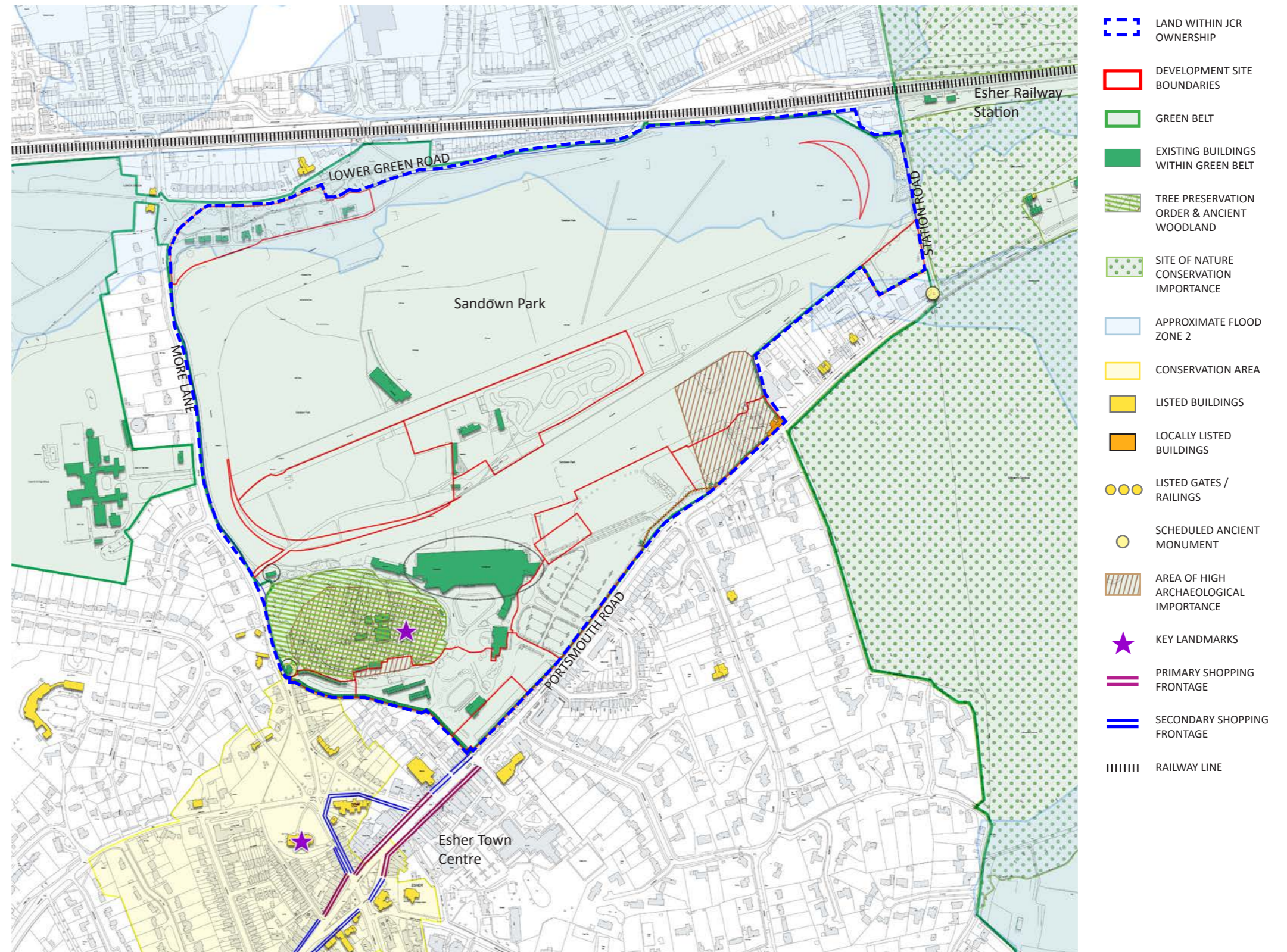


Fig. 36: Diagram illustrating the Constraints of the Masterplan site

5.0 DESIGN PRINCIPLES

5.1 THE OVERALL CONCEPT

The Masterplan seeks to:

- Give careful consideration to the objectives of the Green Belt, in maintaining its openness;
- Focus development areas on the rationalisation of previous developed areas;
- Deliver sustainable development which showcases Sandown Park as a racing and leisure venue and contributes to the Borough's development needs, bringing a number of economic, cultural, social and environmental benefits;
- Enhance connectivity between Esher Town Centre, Sandown Park and Esher railway station;
- Adopt a sensitive approach to the ancient woodland, biodiversity, heritage assets and flood risk, minimising harm wherever possible; and
- Enhance the public realm and green infrastructure links and improve permeability.
- Opportunities to improve integration with Esher Town centre.

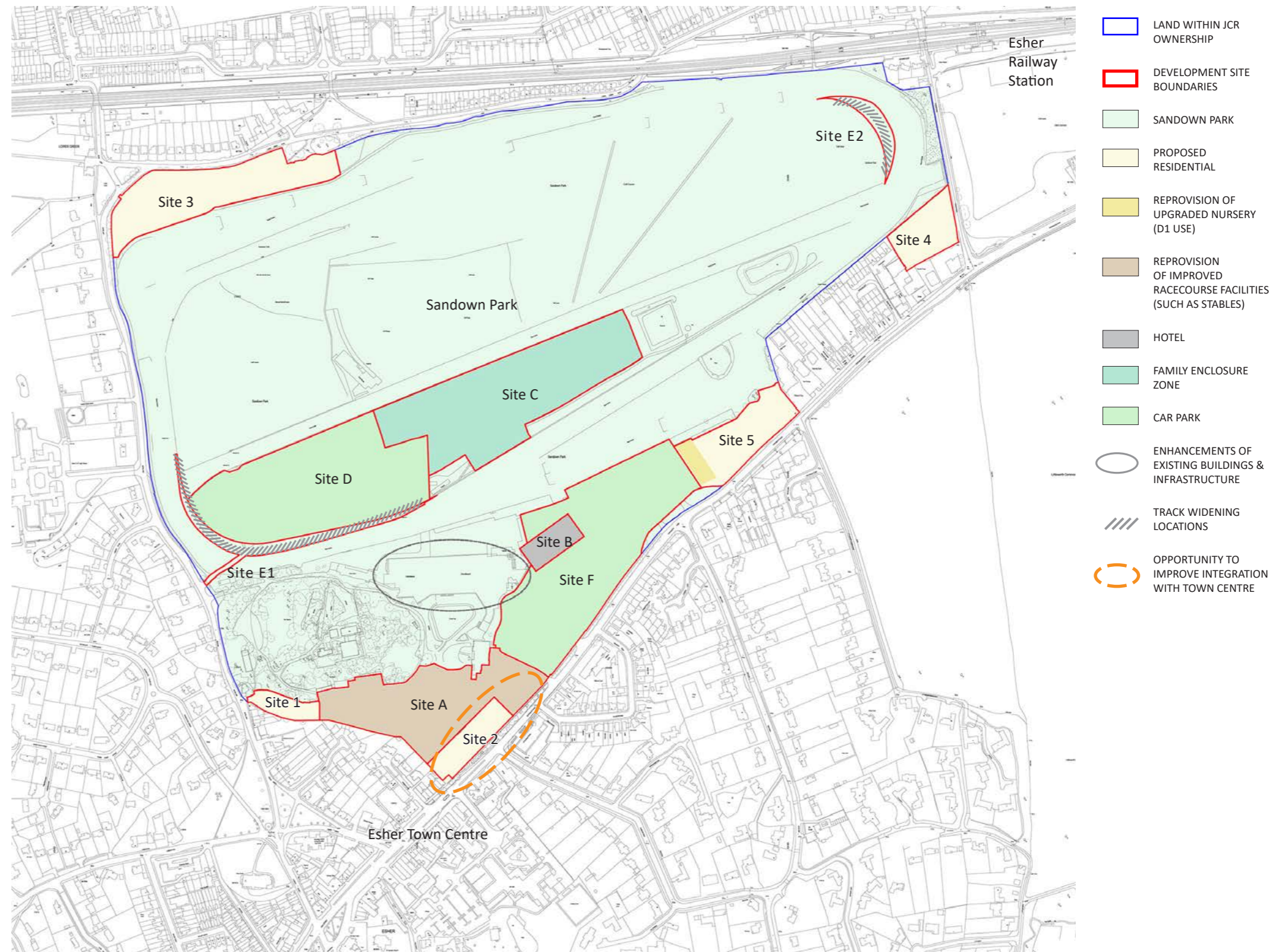


Fig. 37: The Indicative Masterplan

6.0 CONSULTATION

6.1 PUBLIC CONSULTATION

Two Public Exhibitions were held at Sandown Park on December 15th 2018 and January 25th 2019 to invite local residents and other interested parties to view the initial concept ideas for the masterplan.

The total turnout was roughly 400 people, who provided feedback via forms at the venue. Full details of the event are outlined in the Statement of Community Involvement with a summary provided here:

Positive feedback included:

- Update and sustain Sandown Park as asset for Esher
- Improve family facilities, particularly the new area in the centre of the park
- Introduction of business class hotel
- Provide a boost to the High Street
- Retain / provide new jobs
- The need for new and smaller homes
- Improvements to the frontage of Sandown Park (particularly adjoining the High Street)

Feedback requiring further attention included:

- Traffic congestion and safety, including parking-related issues, around the racecourse
- Issues with pedestrian journeys to/from the station – particularly Station Road/ Portsmouth Road to the High Street, and along the northern boundary of the racecourse
- Scale/height of developments
- Privacy for residents near Site 1, Site 2 and the proposed new pedestrian link
- Value of openness and greenness of Sandown Park

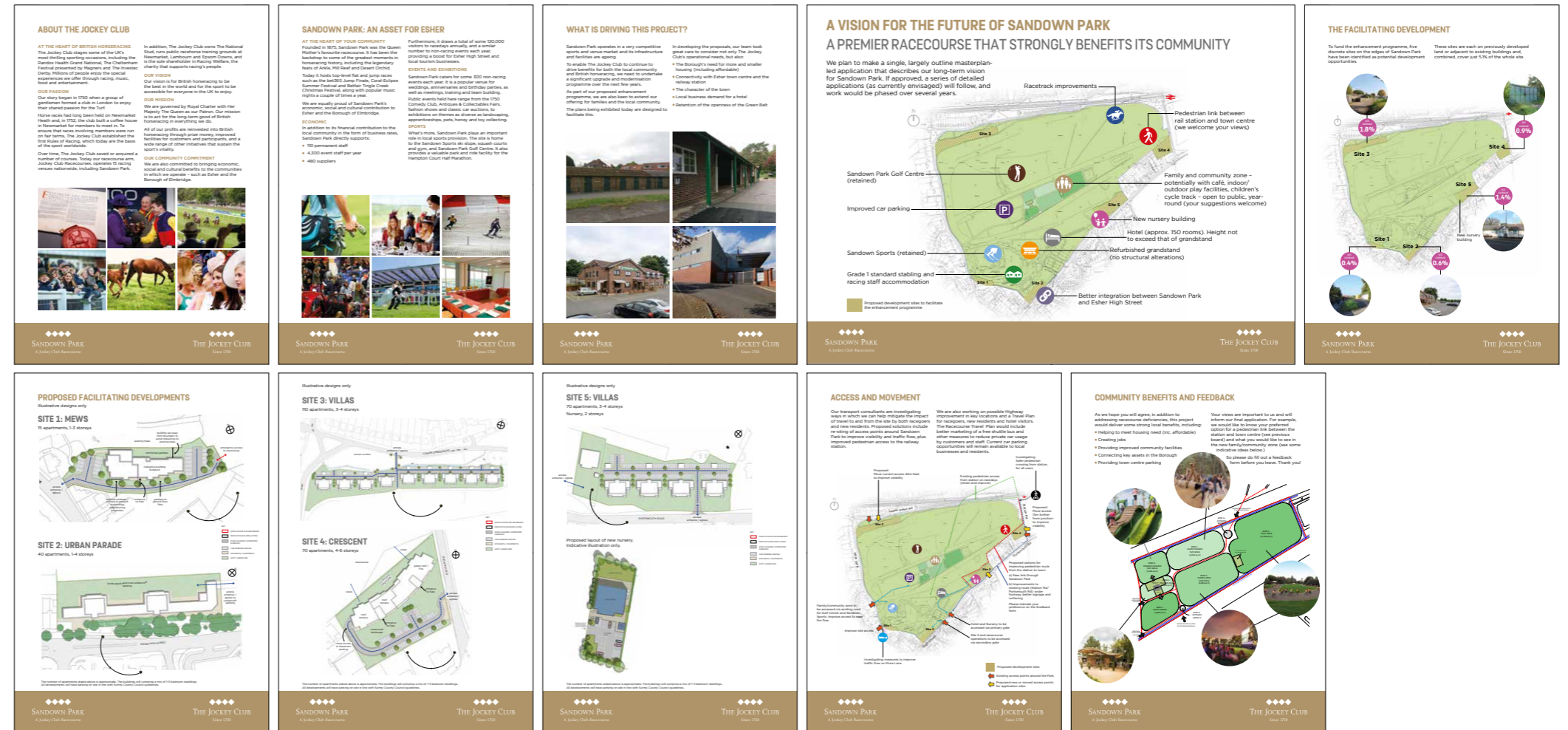


Fig. 38: Images of the exhibition boards from first exhibition

7.0 PROPOSED DEVELOPMENT

7.1 USE & AMOUNT

Overall Racecourse Site: 66.2 hectares

The Enhancement Sites

Site A

Use: Reprovision of improved racecourse facilities (such as stables)

Site Area: 2.2 hectares

Site B

Use: circa 150 room new hotel

Site Area: 0.3 hectares

Site C

Use: Family Enclosure Zone including cafe and soft play
Leisure & Recreational Use

Site Area: 3.3 hectares

Site D

Use: Rationalisation of the Car Park

Site Area: 3.5 hectares

Site E1

Use: Area to facilitate racetrack widening

Site Area: 0.46 hectares

Site E2

Use: Area to facilitate racetrack widening

Site Area: 0.22 hectares

Site F

Use: Optimisation to car parking & main entrance frontage.

Site Area: 3.68 hectares

The Facilitor Sites

Total Number of Residential Units: 318
The residential units are proposed as all apartments in a range of sizes including 1bed, 2bed, and 3 bedrooms.

Site 1

Use: C3 Residential
Site Area: 0.24 hectares
Site Coverage: 0.4 %
Apartments: 15

Site 2

Use: C3 Residential
Site Area: 0.46 hectares
Site Coverage: 0.7 %
Apartments: 49

Site 3

Use: C3 Residential
Site Area: 1.76 hectares
Site Coverage: 2.6%
Apartments: 114

Site 4

Use: C3 Residential
Site Area: 0.57 hectares
Site Coverage: 0.9%
Apartments: 72

Site 5

Use: C3 Residential & D1 Nursery
Site Area: 0.99 hectares
Site Coverage: 1.5%
Apartments: 68

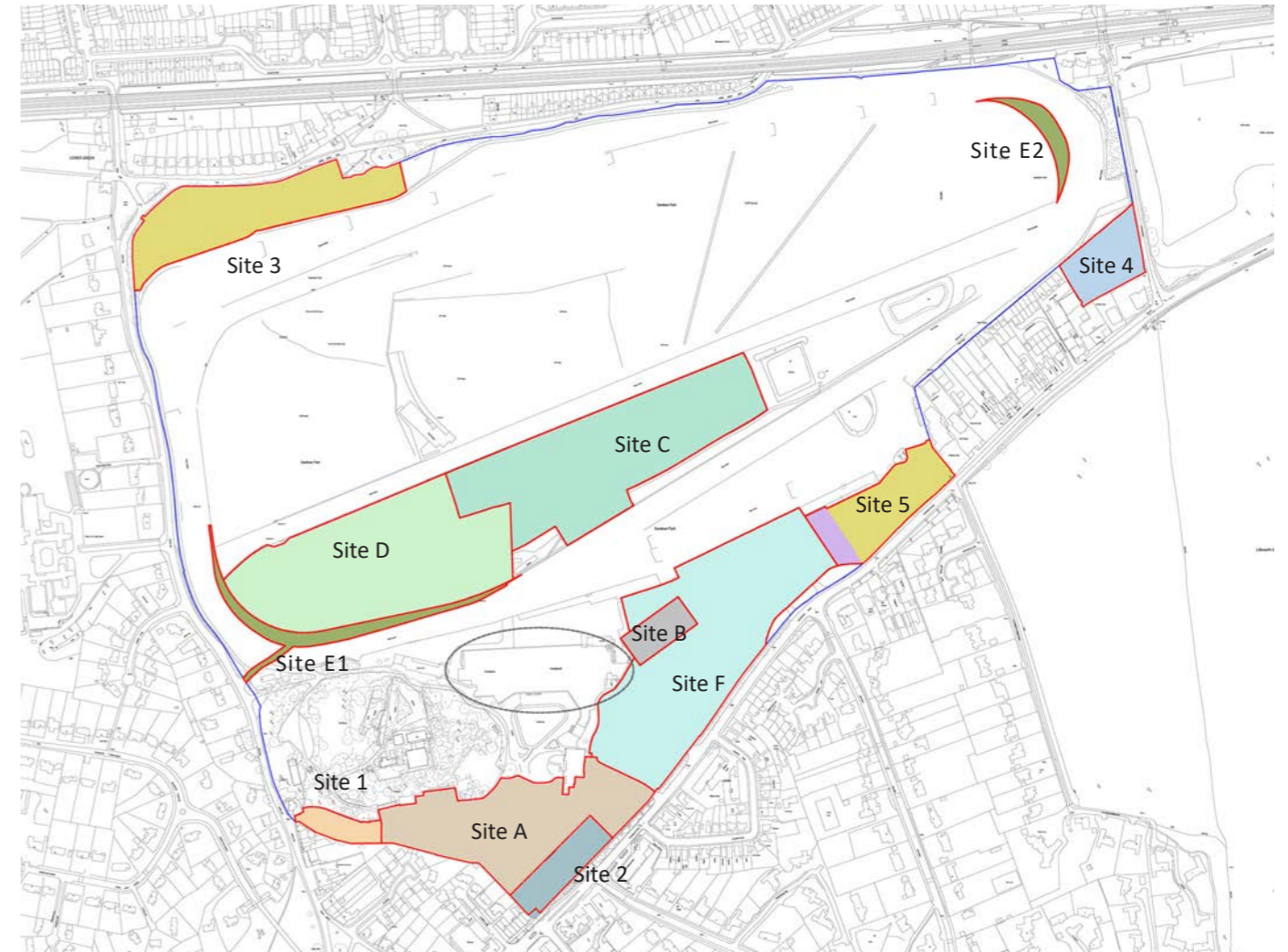


Fig. 39: Masterplan Development Sites highlighted by Land Use

- LAND WITHIN JCR OWNERSHIP
- DEVELOPMENT SITE BOUNDARIES
- RESIDENTIAL MEWS
- RESIDENTIAL VILLAS
- DAY NURSERY OR D1 USE
- RESIDENTIAL CRESCENT
- RESIDENTIAL URBAN FRONTAGE
- REPROVISION OF IMPROVED RACECOURSE FACILITIES (SUCH AS STABLES)
- HOTEL
- FAMILY ENCLOSURE ZONE
- CAR PARK
- TRACK WIDENING
- OPTIMISATION TO CAR PARKING & MAIN ENTRANCE FRONTAGE
- IMPROVEMENTS AND ENHANCEMENTS OF EXISTING BUILDINGS & INFRASTRUCTURE

7.0 PROPOSED DEVELOPMENT

7.2 ACCESS & MOVEMENT

Masterplan access and movement

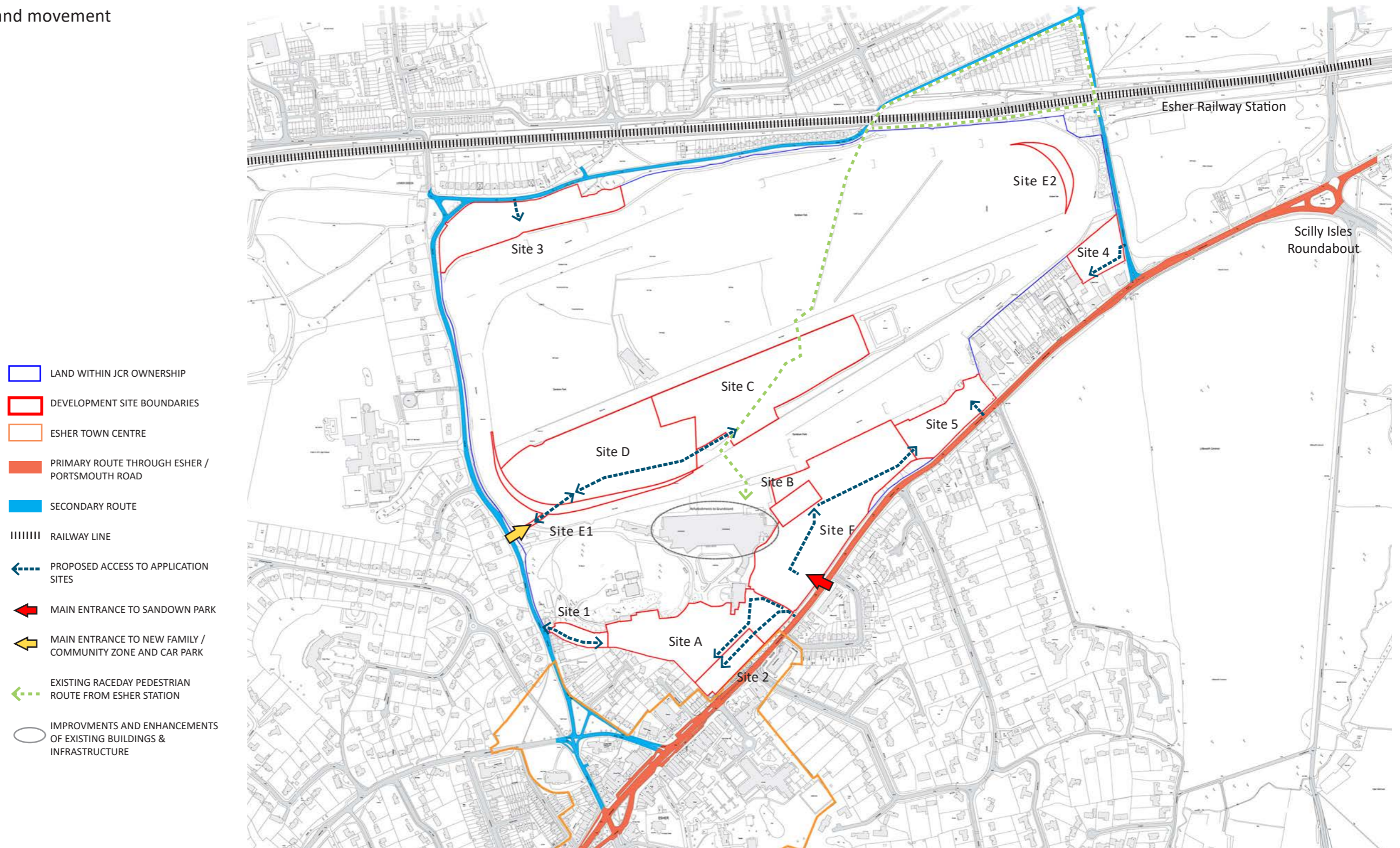
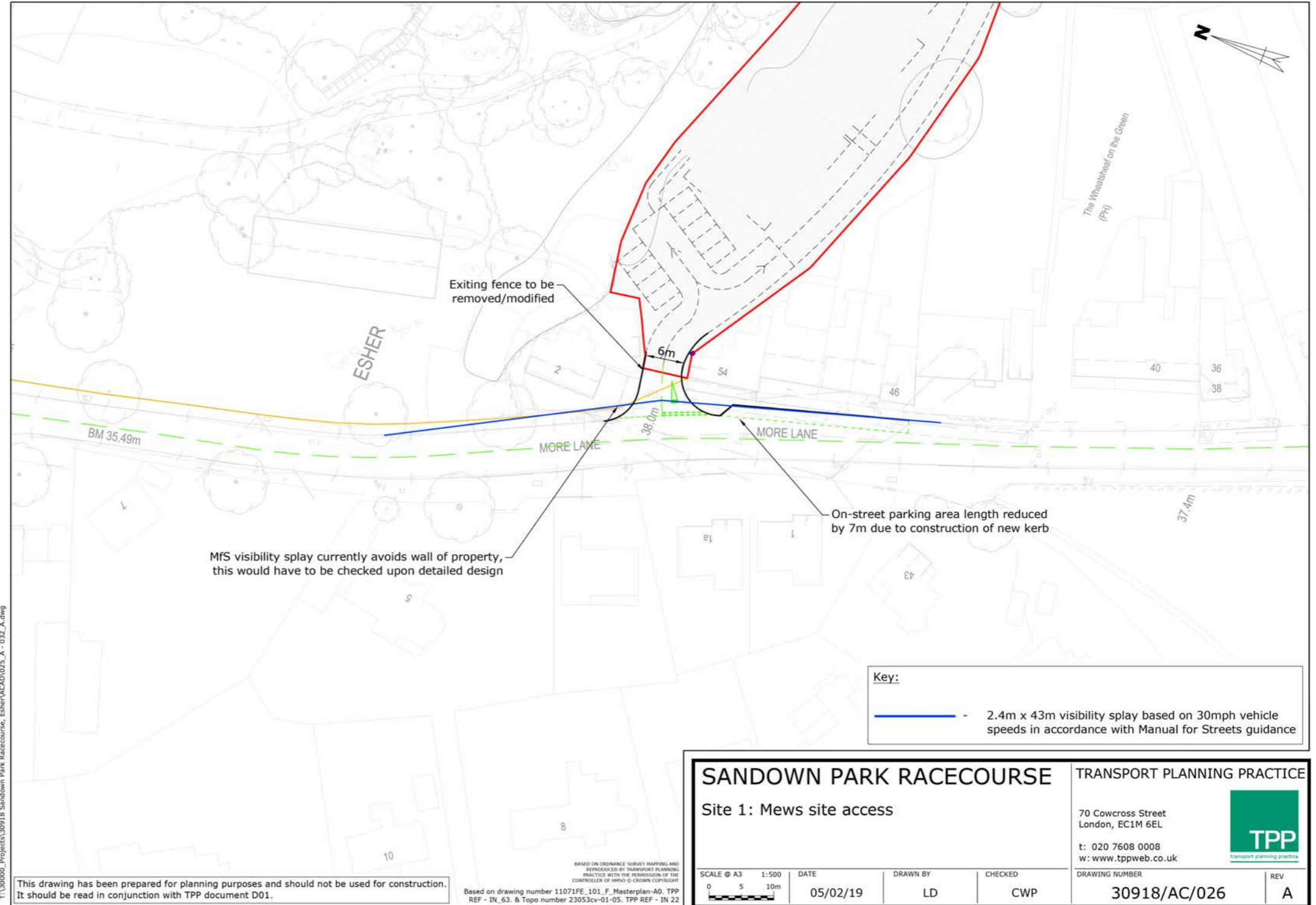


Fig. 40: Masterplan Access and Movement Diagram

7.0 PROPOSED DEVELOPMENT

7.2 ACCESS & MOVEMENT

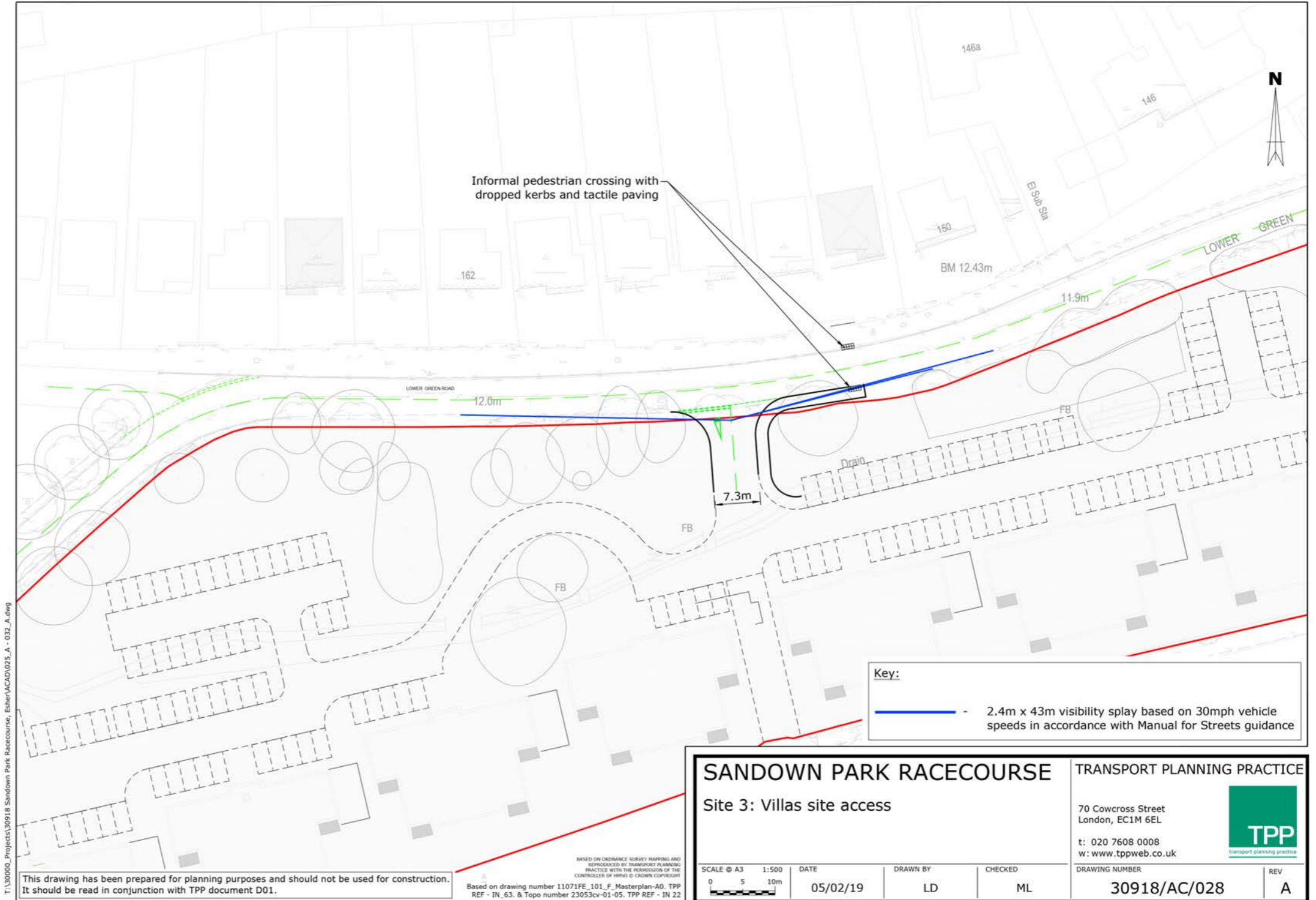
Site 1



7.0 PROPOSED DEVELOPMENT

7.2 ACCESS & MOVEMENT

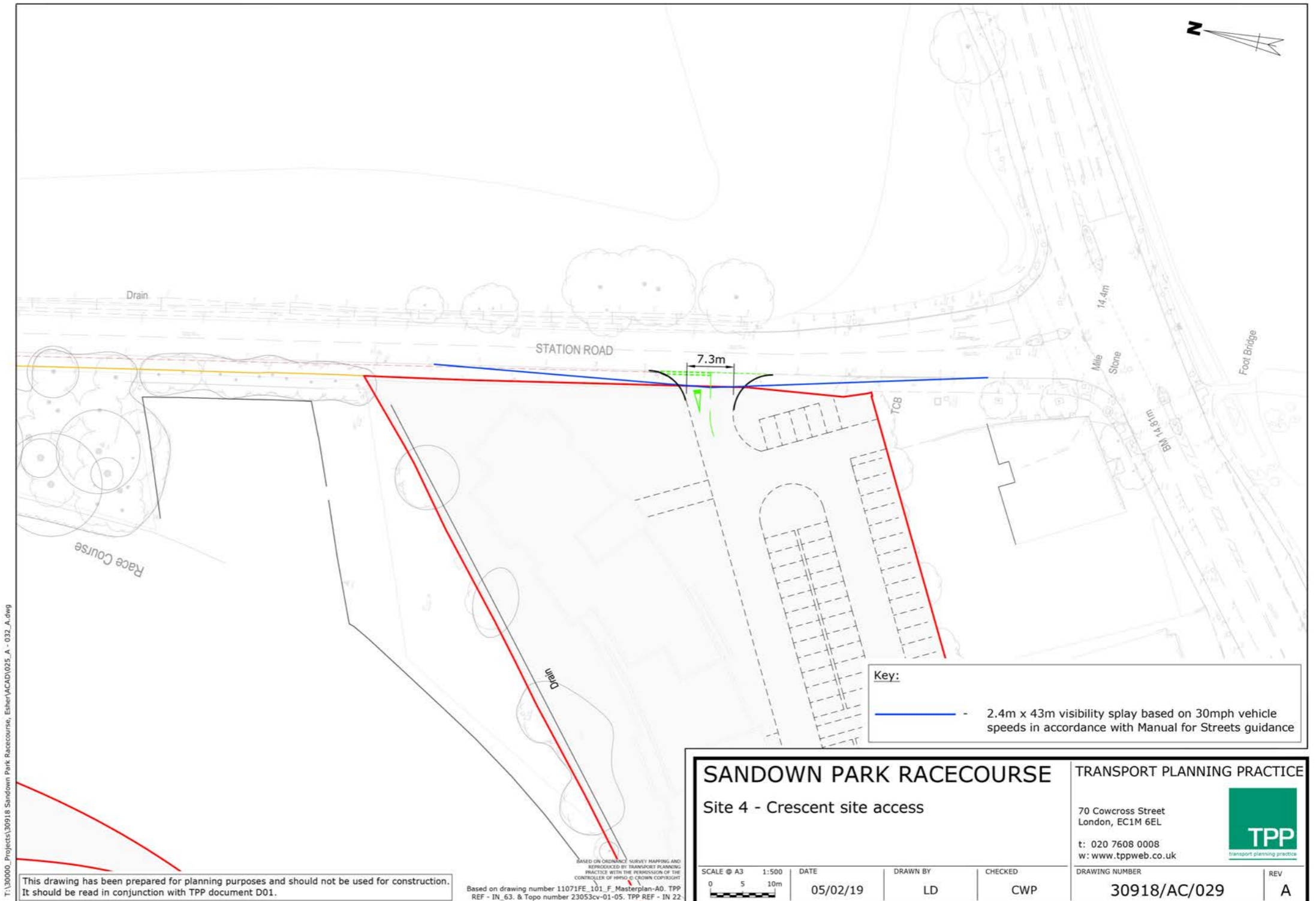
Site 3



7.0 PROPOSED DEVELOPMENT

7.2 ACCESS & MOVEMENT

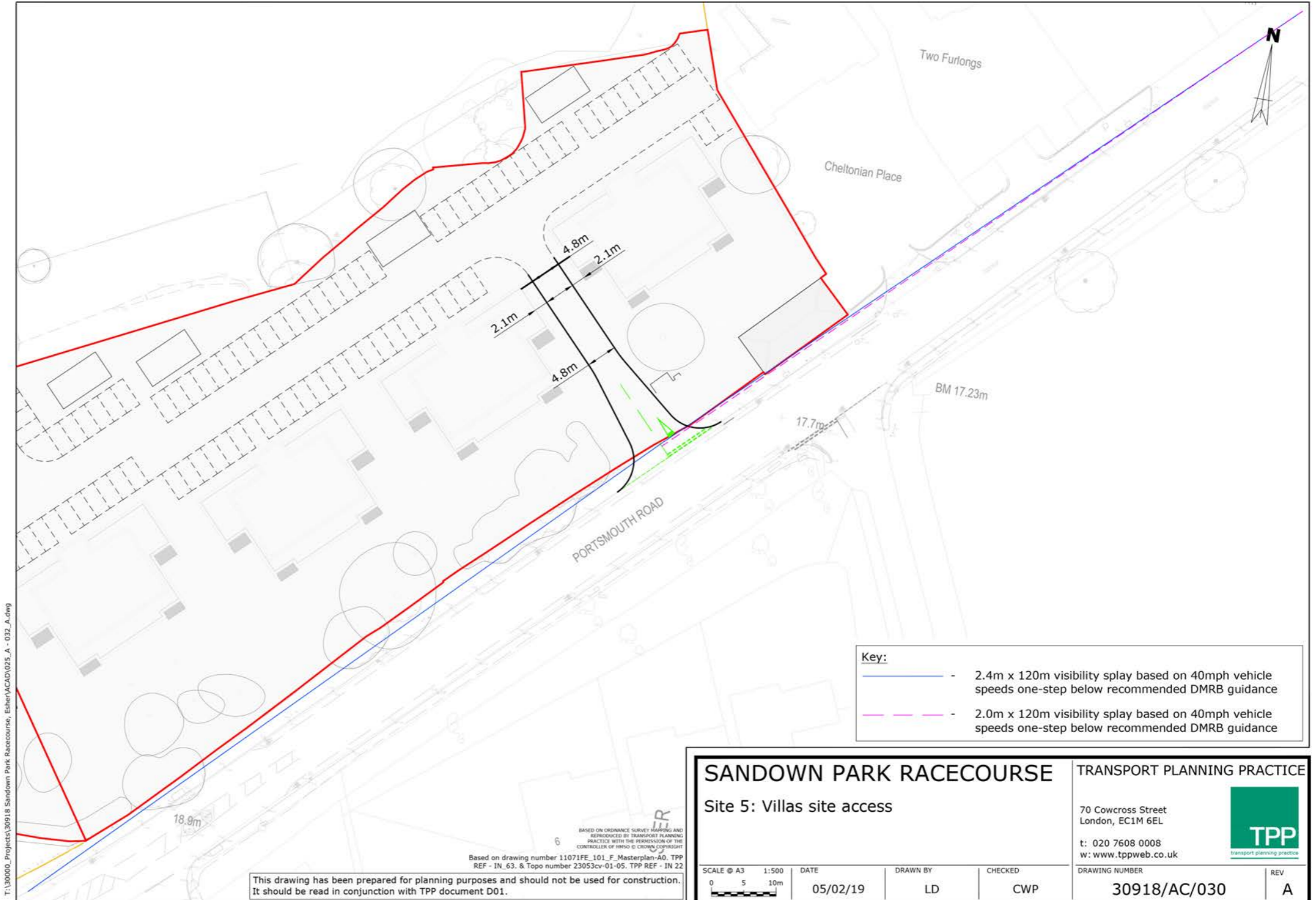
Site 4



7.0 PROPOSED DEVELOPMENT

7.2 ACCESS & MOVEMENT

Site 5



7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 1

Mews Residential

The demolition and rebuilding of the existing stables in a more compact and efficient form, frees up a site area to the east of the proposed new stables, accessed from More Lane.

- An emergency exit will be required from the stables, exiting to More Lane through the created site.
- The building is set away from the heavily sylvan area to the north (The Warren).
- The southern frontage is set away from the houses and gardens to the south
- Outlook from the proposed flats is controlled to minimise overlooking neighbouring properties.
- This layout proposes five x 1-bedroom flats and ten x 2-bedrooms flats, with 21 associated car parking spaces.

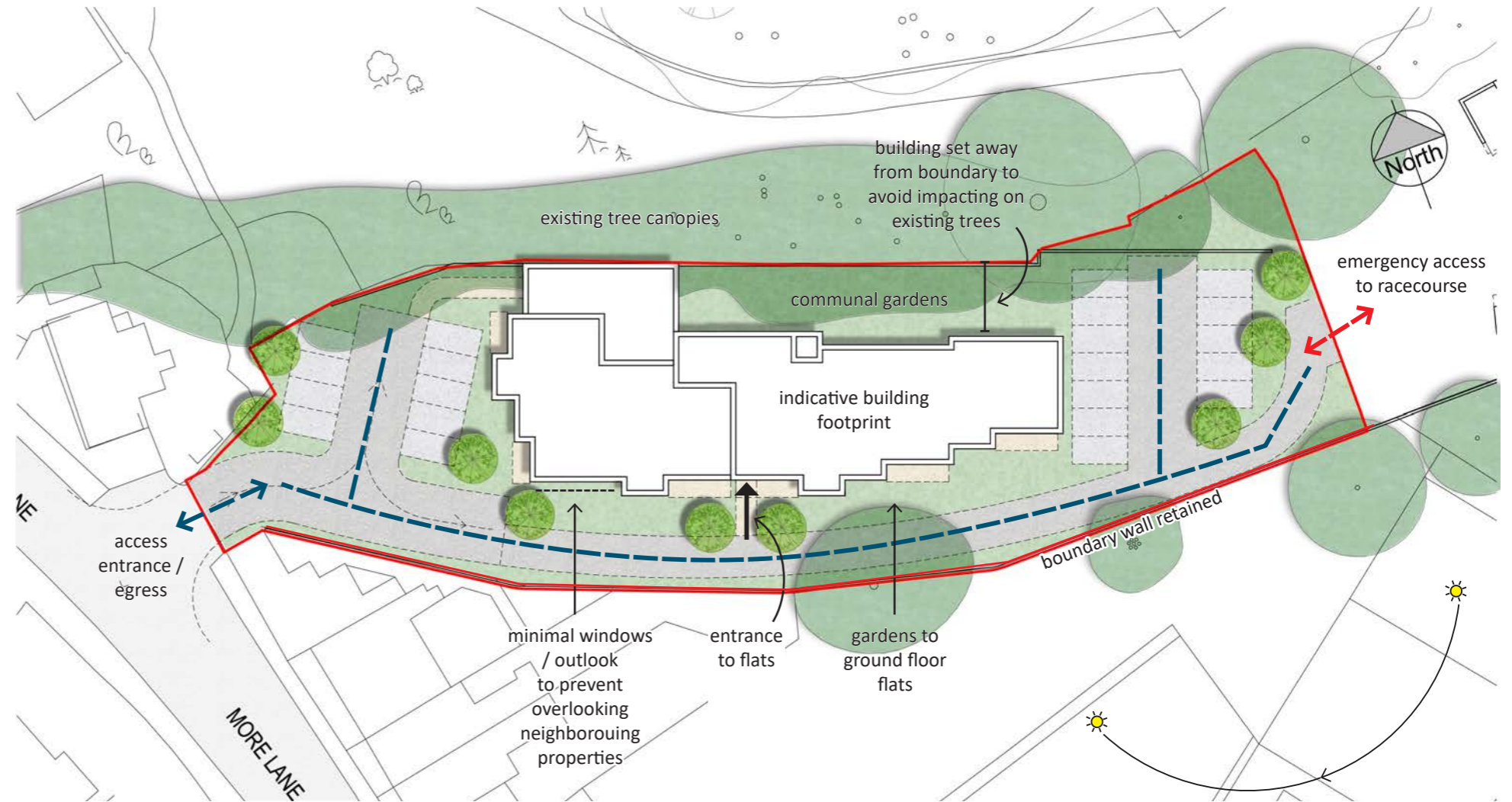


Fig. 41: Illustrative Layout for Site 1 - Mews Residential

KEY	
	APPLICATION SITE BOUNDARY
	INDICATIVE BUILDING SITING
	ROAD (SHARED 'HOMEZONE' SURFACE)
	CAR PARKING SPACES
	PATHWAYS / PAVEMENTS
	SOFT LANDSCAPE
	NEW TREES
	EXISTING TREES RETAINED

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 1

Refuse Strategy

- The refuse vehicle will be able to access and leave the site in forward gear.
- The western parking area will provide the required turning head, reversing to the footpath
- The bin store is circa 10m from the refuse vehicle collection point.

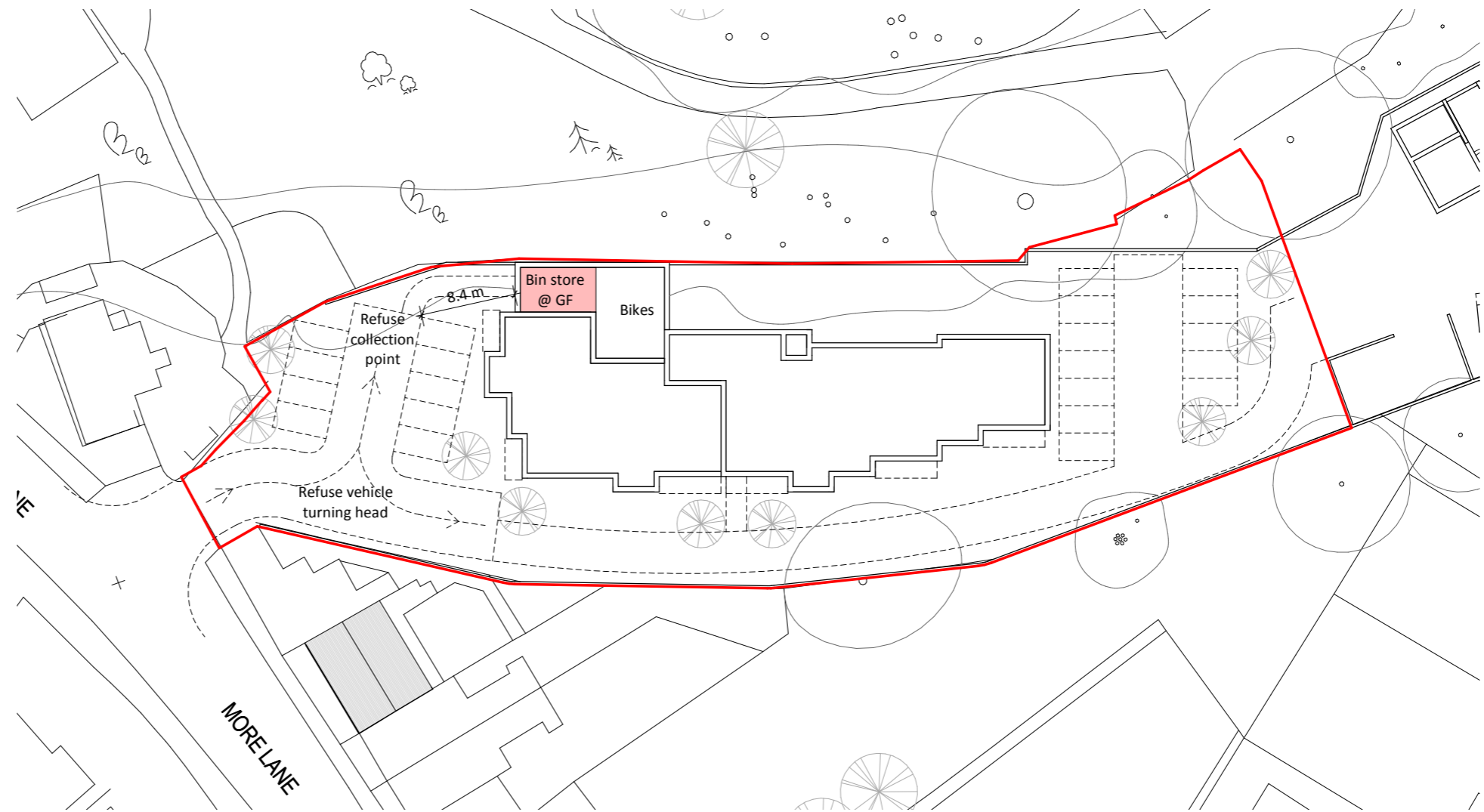


Fig. 42: Illustrative Refuse Strategy Layout Site 1

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 2

Residential Urban Frontage

- A key driver in designing the layout of this proposal is to create a coherent urban extension to the High Street and bring the presence of and interaction with Sandown Park closer to the High Street.
- The existing boundary fencing, line of trees (within the site) and banking will be removed to open up the site and bring the proposed building and landscaping down to interact at street level. The building is set back from the High Street and this space will include a new landscaped pedestrian route into Sandown Park up to the Grandstand and proposed hotel (Site B), with the entrance as close to the High Street as possible.
- Facing the High Street, the proposed building will be 3 storeys to match the adjacent buildings. The car parking will be hidden from view behind the flats facing the High Street in an undercroft parking area.
- The flats facing into Sandown Park overlook the landscaped podium deck over the undercroft car parking. Elements of this elevation will be 1 storey higher, including the end of the building facing the new open park / parking area creating a focal point to the 'new' end of the High Street.
- Tree screening will be introduced to the south-western boundary adjacent to the rear of the properties on Warren Close, and to the north-western boundary with Site A.
- This indicative layout proposes 4 x 1-bedroom flats, 26 x 2-bedroom flats and 19 x 3-bedroom flats along with 72 associated car parking spaces.

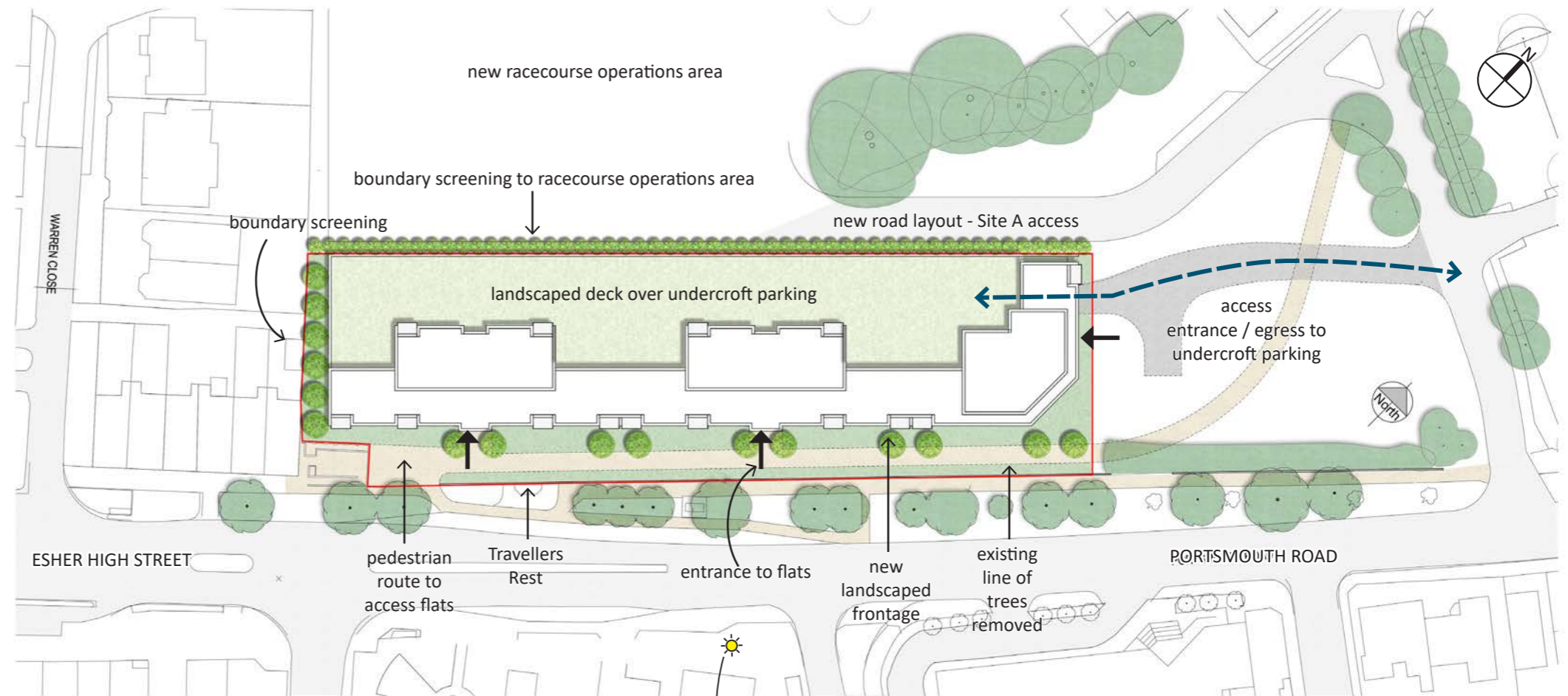


Fig. 43: Illustrative Layout for Site 2 - Urban Frontage

KEY	
	APPLICATION SITE BOUNDARY
	INDICATIVE BUILDING SITING
	ROAD (SHARED 'HOMEZONE' SURFACE)
	LANDSCAPED DECK OVER PARKING
	PATHWAYS / PAVEMENTS
	SOFT LANDSCAPE
	NEW TREES
	EXISTING TREES RETAINED

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 2

Refuse Strategy

- The refuse vehicle will be able to access and leave Sandown Park in forward gear
- This will provide access to the collection point adjacent to the turning area at the northern corner of the building. Building management will move the bins on collection day from the residential bins stores adjacent to each core to the collection point.

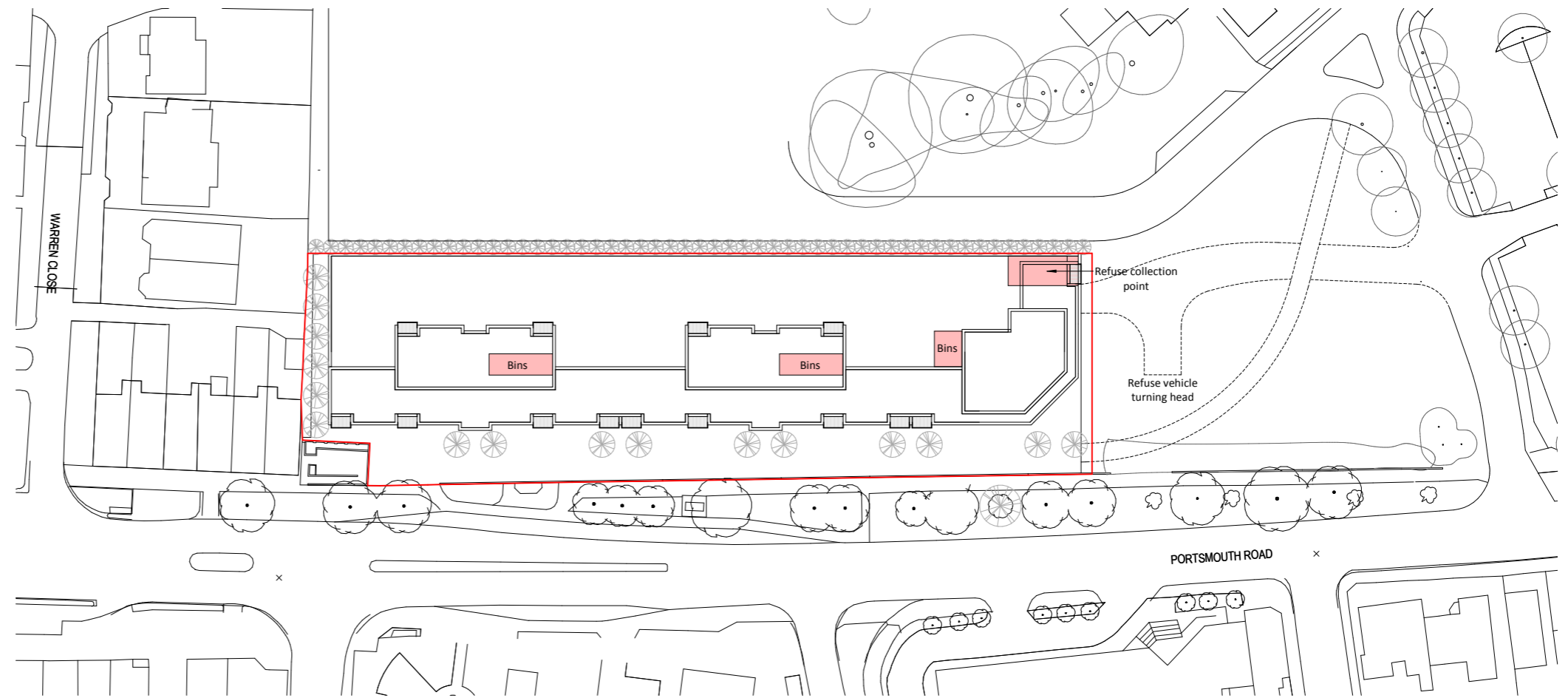


Fig. 44: Illustrative Refuse Strategy Layout Site 2

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 3

Residential Villas

- This area of Sandown Park currently accommodates a number of dwellings used by racecourse staff, and areas used for servicing and storage.
- This proposal will demolish / remove the existing structures to clear a site for 9 'villas', each containing flats over 3 storeys, in a form similar to the larger developments along More Lane.
- The ground floor level will be set at the flood level as denoted in the Flood Risk Assessment. This is up to 1.5m higher than the existing ground levels.
- Associated car parking and site access will be at existing levels so as to retain existing trees and landscaping where possible, and the relationship with Lower Green Road.
- The site access will be relocated further to the east along Lower Green Road, away from the junction with More Lane to help improve safety.
- The area of trees along the south of Lower Green Road will be retained and improved where possible to keep the dense tree screening to the residents to the north of Lower Green Road.
- An emergency access through to the racecourse will be retained opposite the main site access.
- The existing drain running west to east through the site will be culverted or relocated as required.
- This indicative proposal provides 27 x 1-bedroom flats and 87 x 2 bedroom flats with 150 associated car parking spaces.

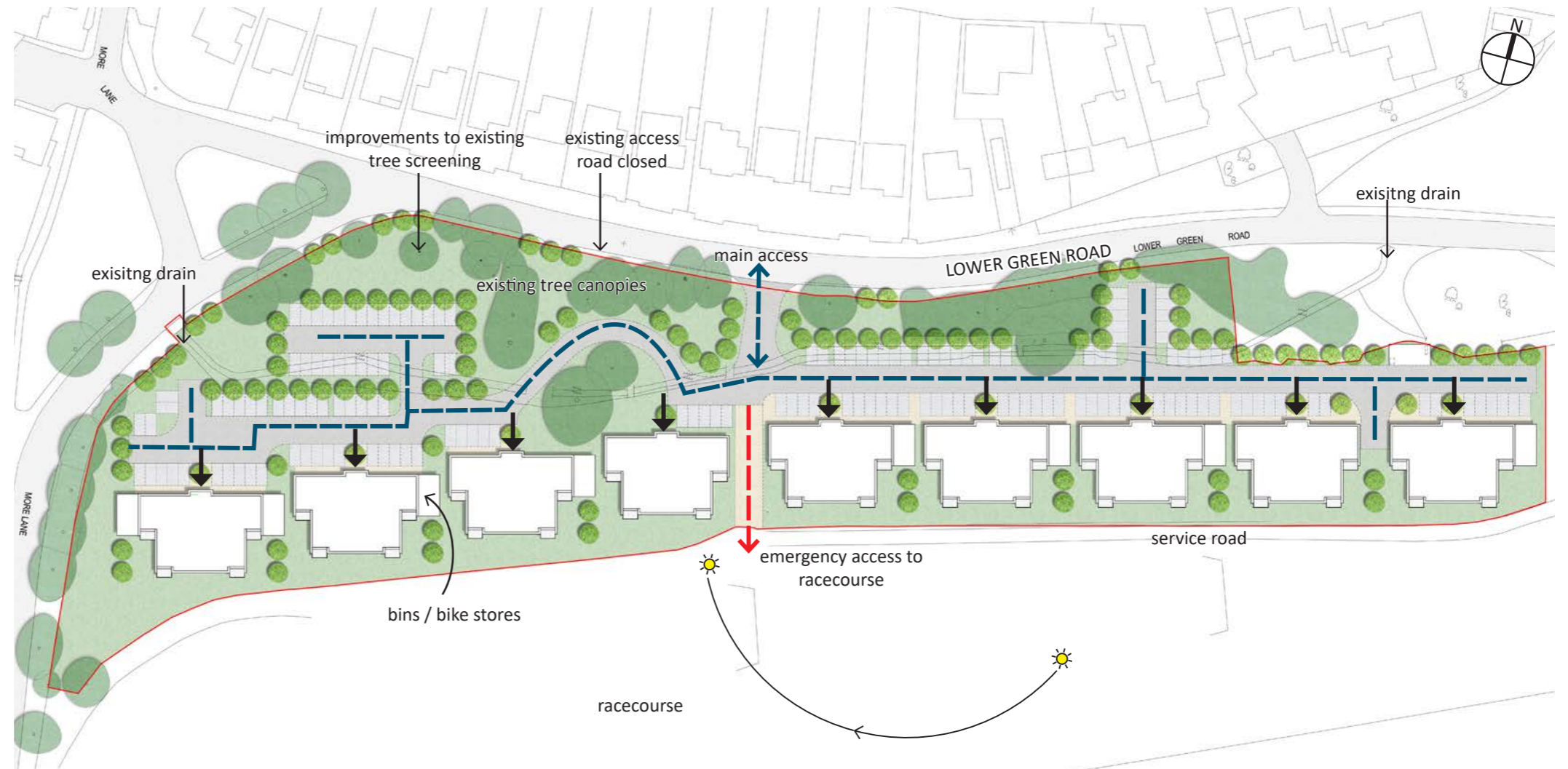


Fig. 45: Illustrative Layout for Site 3 - Villas Residential

KEY	
	APPLICATION SITE BOUNDARY
	INDICATIVE BUILDING SITING
	ROAD
	CAR PARKING SPACES
	PATHWAYS / PAVEMENTS
	SOFT LANDSCAPE
	NEW TREES
	EXISTING TREES RETAINED

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 3

Refuse Strategy

- The refuse vehicle will be able to access and leave the site in forward gear.
- Each 'villa' will access its own refuse store close to the property
- The refuse vehicle will have a turning area at both ends of the site and be able to pull up to within 15m of each bin store.



Fig. 46: Illustrative Refuse Strategy Layout Site 3

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 4

Crescent Residential

- This is currently an infill site to the north of Café Rouge on Station Road.
- The crescent form reflects the sweeping corner of the racecourse.
- The building will be stepped to 4, 5 and 6 storeys.
- Parking will be split between basement level and ground floor, keeping much of the site clear for communal amenity space.
- This indicative proposal provides 2 x studio flats, 39 x 2 bedroom flats and 31 x 3-bedroom flats with associated car parking spaces.



Fig. 47: Illustrative Layout Site 4 - Crescent Residential

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 4

Refuse Strategy

- The refuse vehicle will be able to access and leave the site in forward gear, with a turning head area within the site.
- The bin stores will be located within 15m of the Collection Point, with level access.
- It is assumed there will be a weekly collection.



Fig. 48: Illustrative Refuse Strategy Layout Site 4 - Crescent Residential

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 5

Residential Villas & D1 use (Nursery)

- Development in this area of Sandown Park naturally extends the existing ribbon of built form from Station Road further southwest along Portsmouth Road.
- The existing nursery will be rebuilt and relocated in the western area of the site. Car parking will be allocated and amenity space will be provided within the nursery site demise.
- Access to the nursery will be maintained from the main entrance to Sandown Park.
- The existing nursery buildings and landscaping will be demolished, apart from the original Toll House element which will be renovated and utilized part of the residential development.
- The majority of the site will accommodate 4 new 'villas' each containing flats over 4 storeys, with the fourth floor set back or within a pitched roof.
- The flats will have a south facing aspect, set back from Portsmouth Road overlooking the communal gardens.
- The communal gardens run the length of the boundary with Portsmouth Road and retains much of the existing tree screening with further trees added to help preserve the current perceived density of trees.
- The site boundary will be improved to allow views into the communal gardens from Portsmouth Road.
- The parking area runs along the northern boundary of the site to reduce the visible areas of hard standing as viewed from Portsmouth Road, and screened from the racecourse with new landscaping.
- A new site access is shown at the eastern end of the site from Portsmouth Road.
- This indicative proposal provides 36 x 1-bedroom flats, 24 x 2 bedroom flats and 8 x 3 bedroom flats with 80 associated car parking spaces.



Fig. 49: Illustrative Layout for Site 5 - Villas Residential & Day Nursery or D1 Use

7.0 PROPOSED DEVELOPMENT

7.3 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site 5

Refuse Strategy

- The refuse vehicle will be able to access and leave the site in forward gear.
- Each 'villa' will access its own refuse store close to the property
- The refuse vehicle will have a turning area at the western end of the site and be able to pull up to within 15m of each bin store. The refuse lorry would reverse back a short distance to access the eastern bin store.

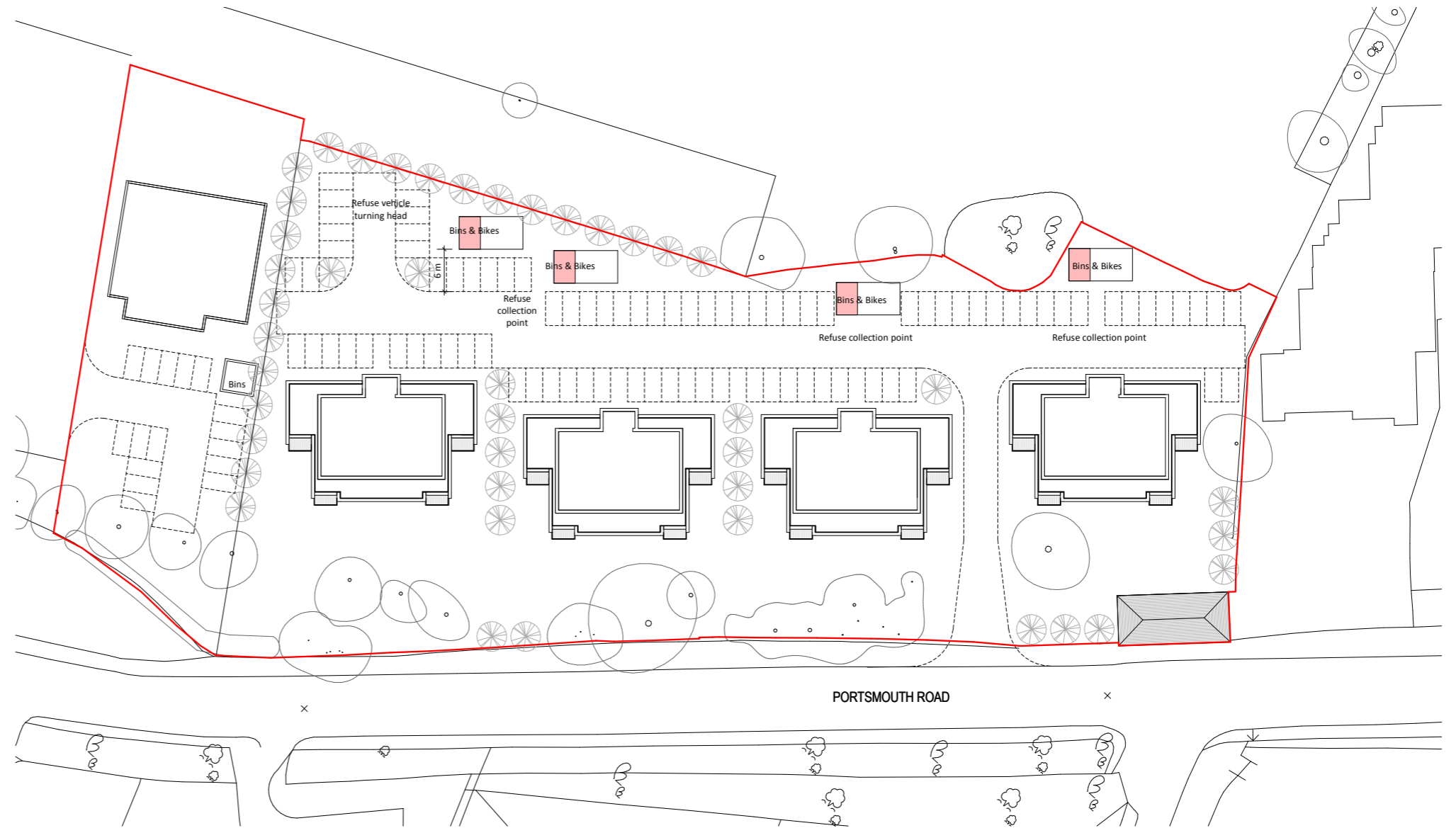


Fig. 50: Illustrative Refuse Strategy Layout Site 5

7.0 PROPOSED DEVELOPMENT

7.1 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site A

Racecourse Operations Area

- It is proposed all the existing facilities in this area will be demolished / removed (stables, hotel, pre-parade ring etc) and replacements built to current British Horseracing Authority standards.
- The pre-parade ring and associated saddling boxes will relocate closer to the main events area.
- The area required for horse box parking and unloading are located along the southern boundary of the site. This will double as standard car parking on non-race days. This will share a boundary with Site 2 and will be screened with hedging and trees.
- A new facility to accommodate race day staff will be located at the northern boundary of the site providing 20 hotel-type rooms at first floor and a canteen and changing facilities at ground floor.
- New stables and associated facilities will accommodate much of the area of the existing stables albeit in an optimised layout.

Refuse Strategy

- Refuse arrangements to be retained as existing or secured at the detailed design stage



Fig. 51: Illustrative Layout Concept for Site A

7.0 PROPOSED DEVELOPMENT

7.1 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site A



Fig. 52: Illustrative Layout Site A

7.0 PROPOSED DEVELOPMENT

7.1 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site B

Hotel

- A business class mid-range hotel is proposed for this area.
- The location close to the existing Grandstand will help with the perception that the hotel is an extension to the Grandstand facilities and part of the local cluster group of buildings.
- The massing of the hotel will extend no higher than the Grandstand roof.
- Access will be via the main gate into Sandown Park and parking will be allocated within the general car parking area.
- The proposal is for a circa 150 bed hotel over 6 storeys. The ground floor will likely consist of lounge, bar, restaurant and back of house / servicing. The floors above contain the bedrooms. The first floor could also include an external terrace with views out across Sandown Park.

Refuse Strategy

- Refuse arrangements to be retained as existing or secured at the detailed design stage

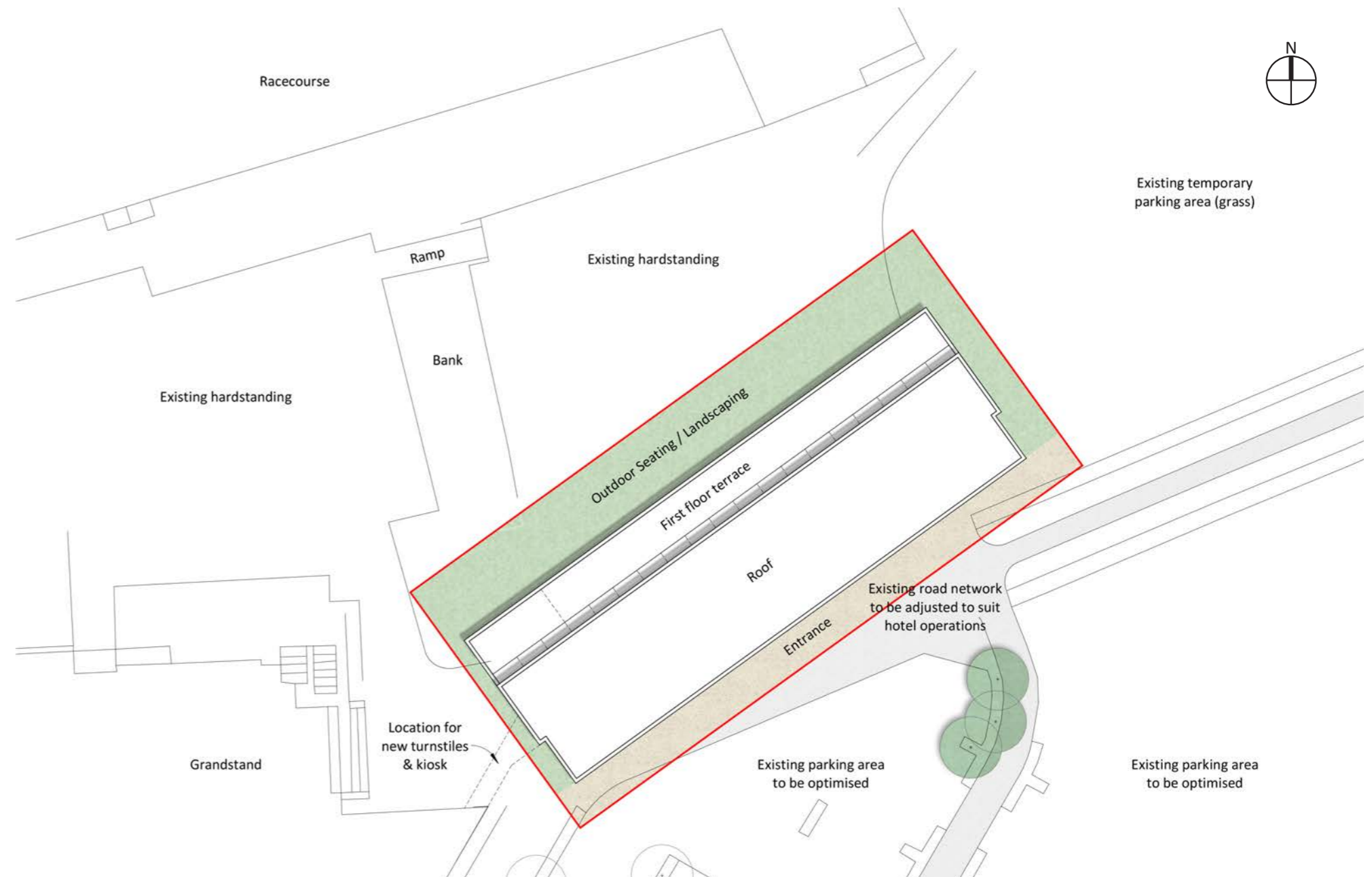


Fig. 53: Illustrative Layout Site B

7.0 PROPOSED DEVELOPMENT

7.1 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site C

Family / Community Zone

- This area of the racecourse is currently used for powered kart racing and consists of a large area of tarmac roadway, bounded by tyre walls, with associated dilapidated out buildings and car parking. All buildings currently on the site will be demolished and the kart track will be cleared of any karting paraphernalia including the tyre walls.
- The proposal retains the kart track as a cycle track and creates a new Family / Community Zone to attract families and children to come racing at Sandown Park, ensuring it is a focal point for the community all year around with a range of attractive family centric facilities and activities.
- The facilities include a number of new outdoor spaces including an open park, children’s play, viewing area and a cycle track for children. A café provides much needed facilities and a focus for the proposal, consisting of a ‘soft play’ café for families and young children, and an additional linked café associated with the open park and viewing area.

- The main entrance to the proposal from the adjacent car park area is via the café. This provides secure access to the younger children’s soft play and external play areas. The adjacent café works in tandem with the soft play café with shared kitchen / servicing facilities, but is separated from the soft play providing an alternative quieter venue.
- The central area of the site provides a large open family / community park for race viewing, outdoor cinema (big screen), and other larger scale landscape features (such as sunken amphitheatre). Adjacent to this is a larger play area for older children, easily accessible and overlooked by the parents in the park and the café.
- The furthest area beyond the park is the cycle track which provides the community with an activity area for young children to learn to cycle, all year round. Additionally, the track would be available for any appropriate alternative use, such as Segways, skate / bmx park, or possibly karting for younger children with low power battery powered karts.

Refuse Strategy

- Refuse arrangements to be retained as existing or secured at the detailed design stage

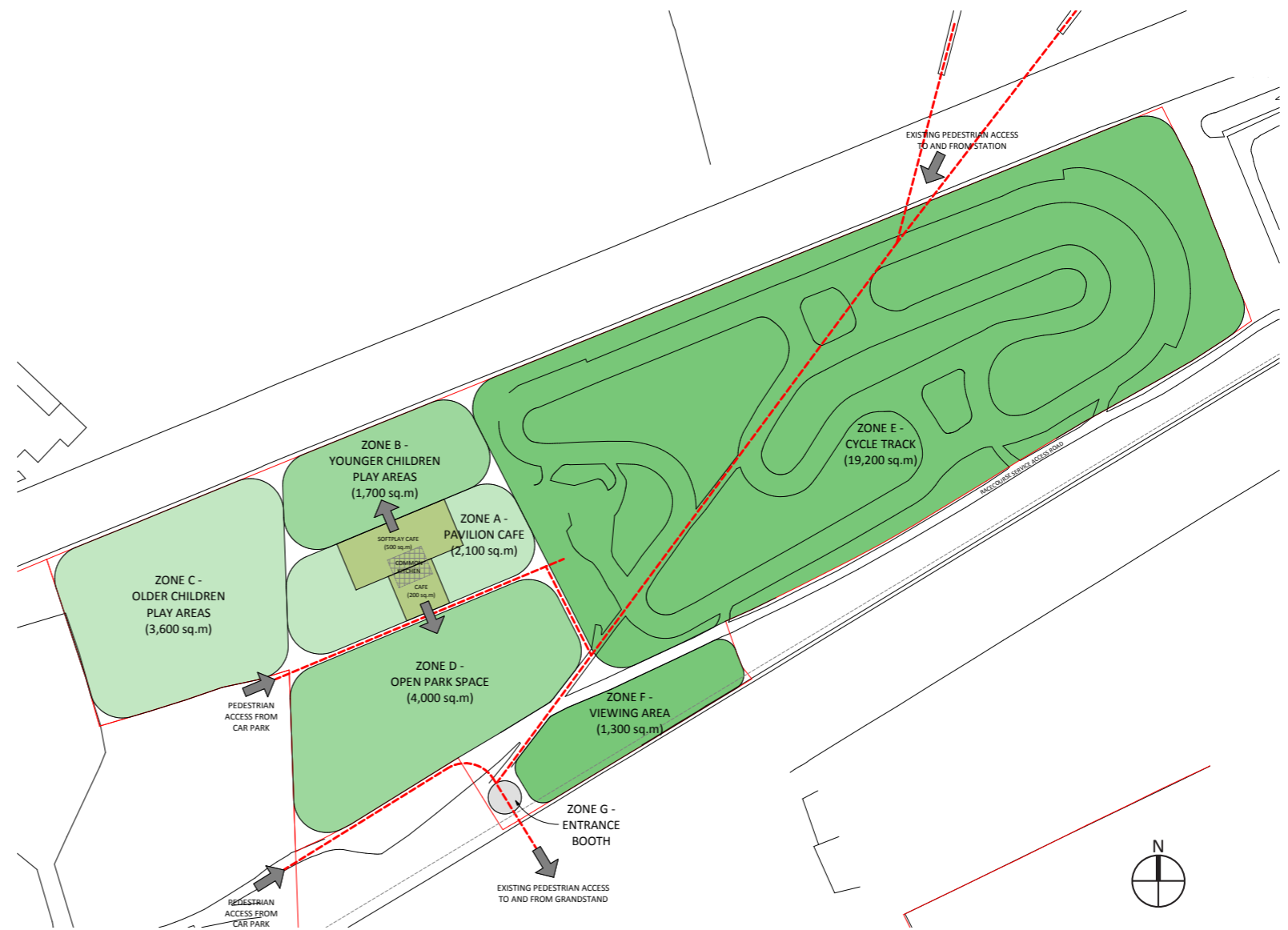


Fig. 54: Illustrative Layout Site C

7.0 PROPOSED DEVELOPMENT

7.1 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site D

Rationalised Car Parking Area

- Site D is currently used for overflow car parking for high capacity race days, and parking for the golf course visitors will be retained and optimised.
- The existing hardstanding area near the golf course will remain, with an additional area added close to Site C for the Family / Community Zone.
- The remaining car parking area is currently grass cover which is not always practical for use as car parking, especially in wet weather. The proposal is to add surfacing where required to help with vehicular access throughout the site, while retaining the greenness of the open space as far as possible.

Refuse Strategy

- Refuse arrangements to be retained as existing or secured at the detailed design stage



Fig. 55: Illustrative Layout Site D

7.0 PROPOSED DEVELOPMENT

7.1 INDICATIVE LAYOUT, LANDSCAPE & REFUSE

Site F

Optimised Parking & Sandown Park Frontage

- The area will remain in use as car and coach parking for Sandown Park, with optimization in the layout and improvements to the landscaping and surfacing where required.
- In addition there will be:
 - » boundary improvements along Portsmouth Road
 - » adjustments to the area in front of the hotel to accommodate the hotel entrance and drop-off circulation
 - » a new viewing area in front of the hotel to extend the Grandstand viewing area
 - » reinforcement to the areas of grass parking
 - » a new turnstile kiosk will be constructed (as demolished in Site B), to be located near the Grandstand
 - » a new broadcasting compound will be constructed (as demolished in Site B), to be located near the Grandstand
 - » a new electrical Ring Main Unit is required within Site F, the location of which will be advised by UKPN.

Refuse Strategy

- Refuse arrangements to be retained as existing or secured at the detailed design stage



Fig. 56: Illustrative Layout Site F

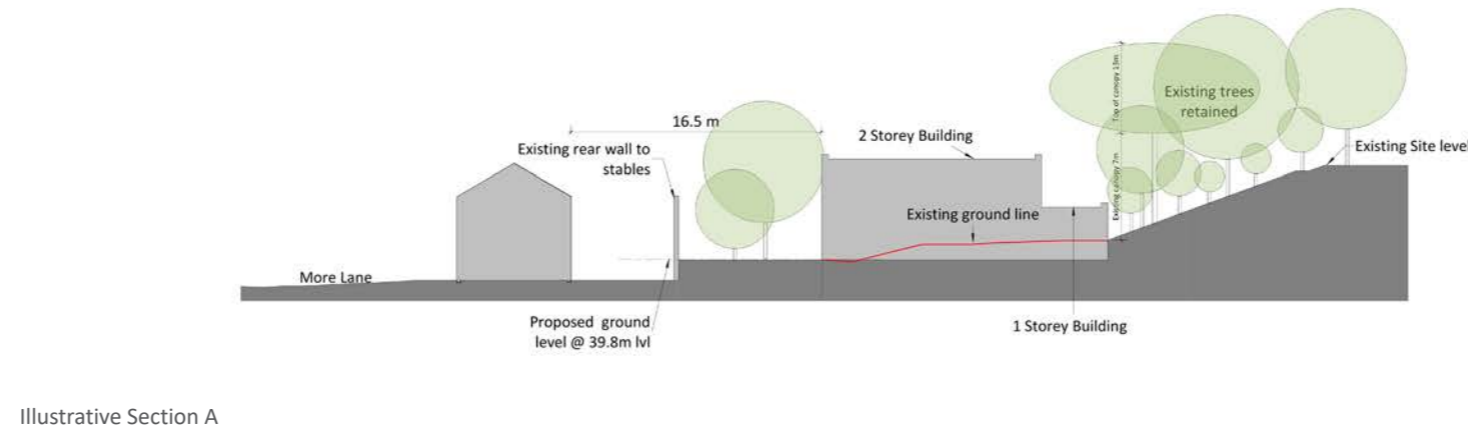
7.0 PROPOSED DEVELOPMENT

7.2 INDICATIVE SCALE & APPEARANCE

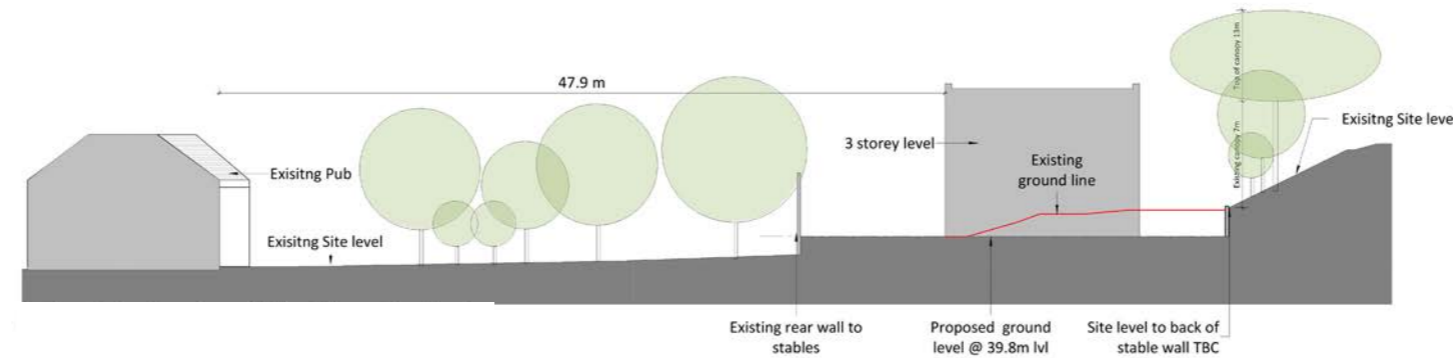
Site 1

Mews Residential Apartments

- The indicative building heights are split into two elements, one at 2 storeys and one at 3 storeys
- The 2 storeys element is located at the western end of the site minimising visual impact from More Lane and the existing houses.
- Currently it is assumed the development will have a flat roof to keep the massing as low as possible (as the sections indicate), however the design could accommodate pitched roofs which may help reduce the perceived building height by accommodating the top floor within the roof with dormer windows. The roof could be either a mansard form or raised half a storey to avoid loss of internal floor area.
- Section 1 shows the relationship of the proposed scale with the closest existing property (54 More Lane). Retaining the existing rear wall to the stables will help screen the proposal from this property.
- Section 2 shows the building is over 45m from The Wheatsheaf pub on More Lane, with much of the massing screened by the retaining wall.



Illustrative Section A



Illustrative Section B

Fig. 57: Illustrative sections through Site 1

KEY	
	APPLICATION SITE BOUNDARY
	1 STOREY
	2 STOREYS
	3 STOREYS

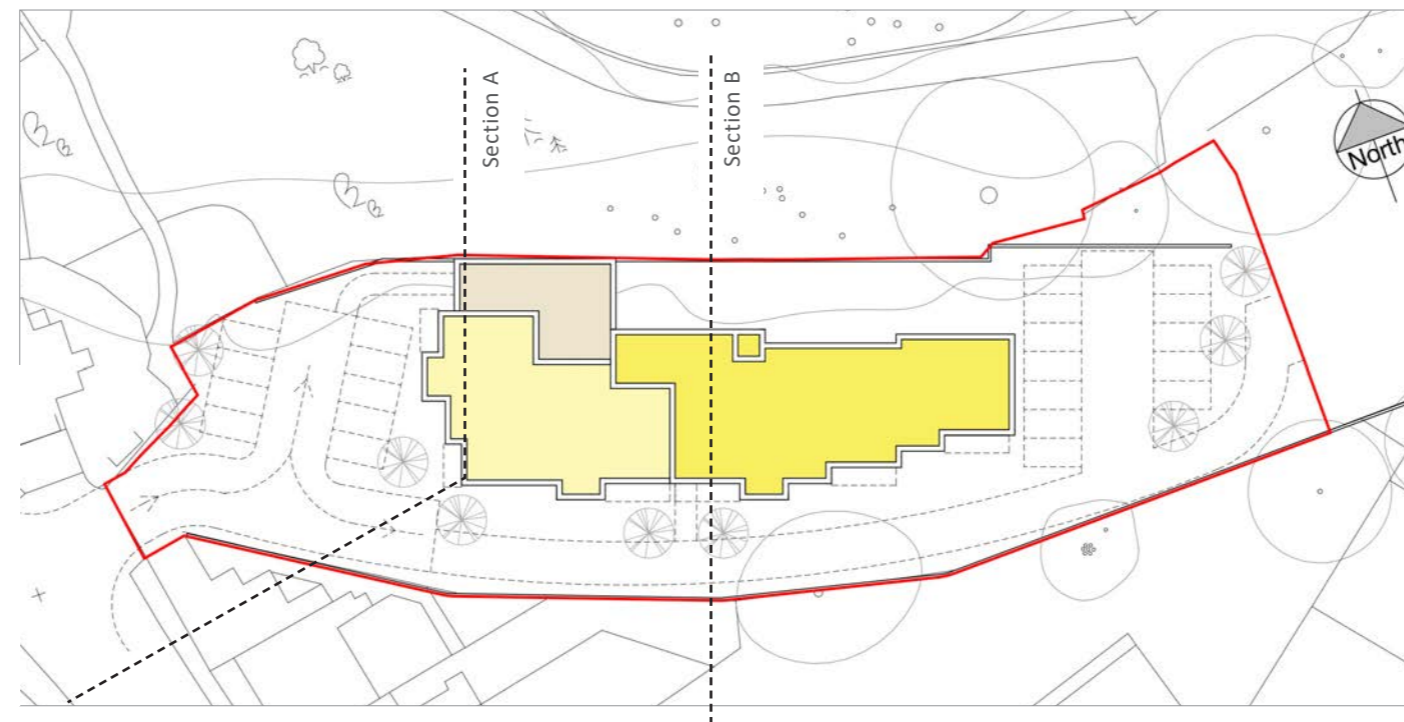


Fig. 58: Illustrative Layout showing the buildings heights



Fig. 59: Illustrative Appearance - precedent images

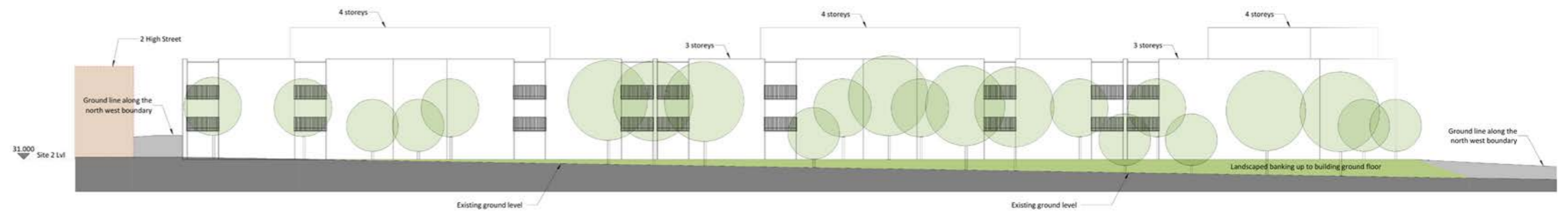
7.0 PROPOSED DEVELOPMENT

7.4 INDICATIVE SCALE & APPEARANCE

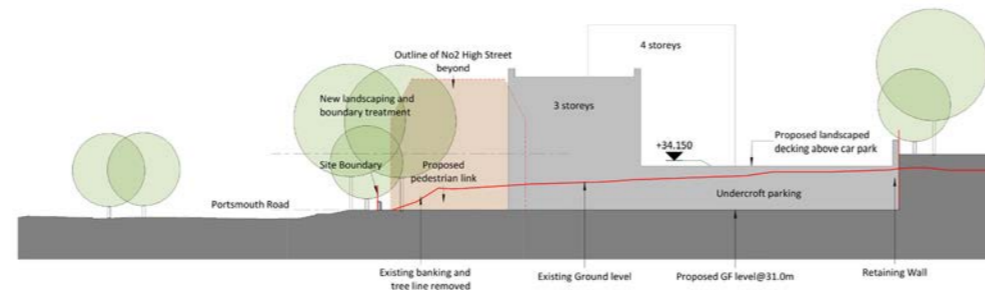
Site 2

Urban Frontage Residential Apartments

- Facing the High Street, the proposed building will be 3 storeys to match the scale of the existing adjacent buildings on the High Street.
- Facing Sandown Park the building will be predominantly 2 storeys above the podium deck with elements of 3 storeys. The podium deck is 1 storey above High Street level.
- Materials will be used to match the local brick, and the local vernacular will influence the design to ensure the building visually blends into the High Street.



Illustrative Section A



Illustrative Section B

Fig. 60: Illustrative sections through Site 2

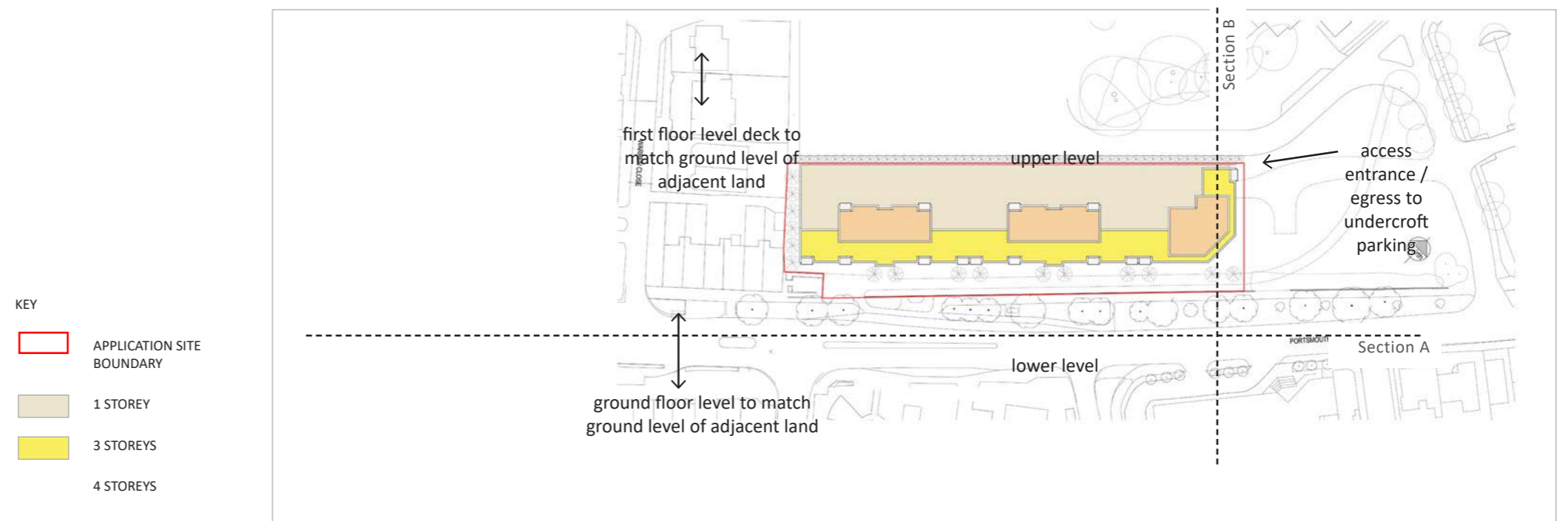


Fig. 61: Illustrative Layout showing the buildings heights

7.0 PROPOSED DEVELOPMENT

7.4 INDICATIVE SCALE & APPEARANCE

Site 3

Villas Residential Houses

- Each 'villa' is 3 storeys and currently proposed to be of flat roof form, however the design could accommodate pitched roofs which may help reduce the perceived building height by accommodating the top floor within the roof with dormer windows. The roof could be either a mansard form or raised half a storey to avoid loss of internal floor area.
- The buildings are narrower facing the racecourse to the south and wider facing north.
- Section 1 shows the relationship of the proposed scale with the closest existing property on More Lane (number 53). There is a clear similarity in scale at this point.
- Section 2 shows the relationship of the proposed scale with the closest existing property on Lower Green Road (number 152). The proposed building here is circa 49m from number 152, with a dense screen of tall trees and bushes along the boundary.

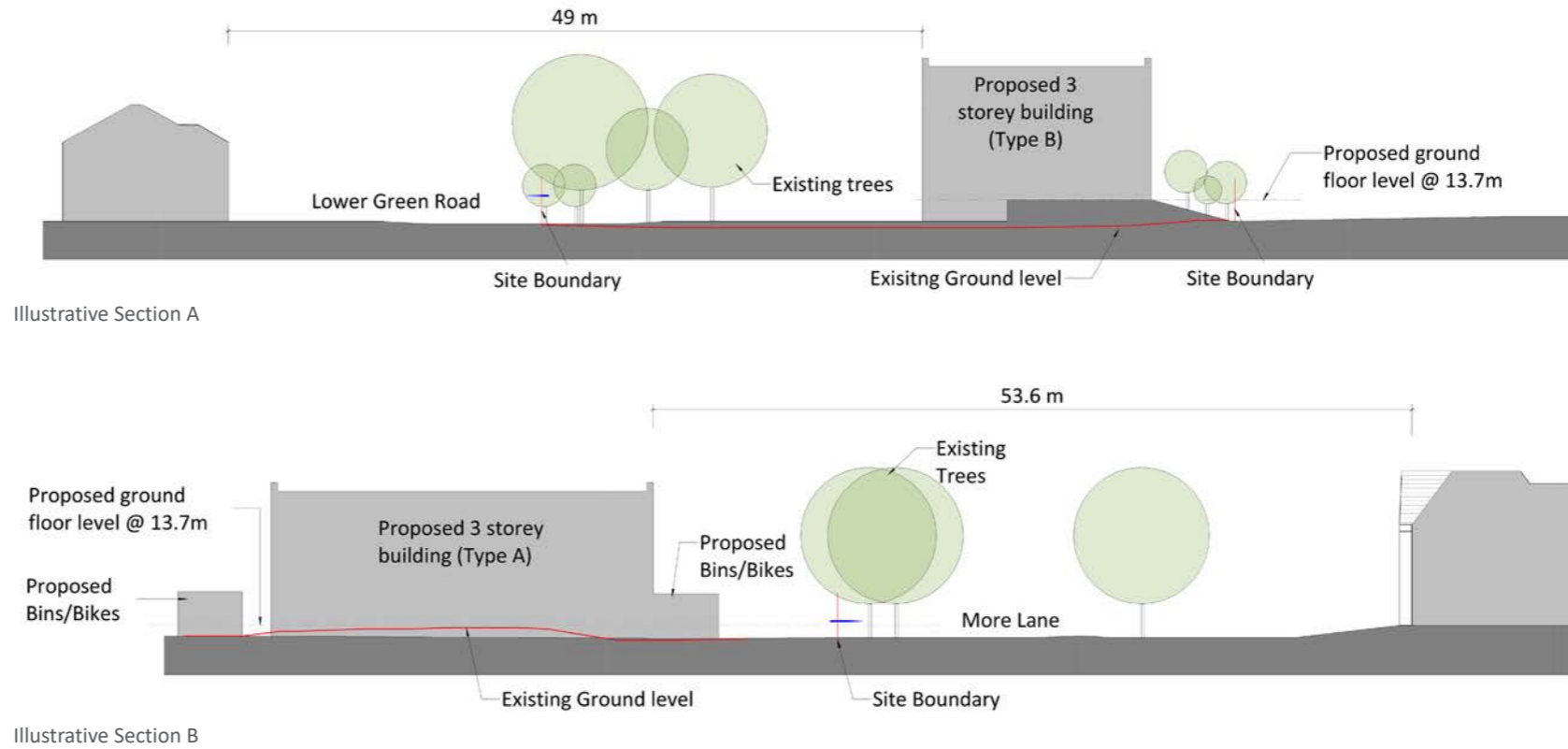


Fig. 62: Illustrative sections through Site 3



Fig. 63: Illustrative Layout showing the buildings heights



Fig. 64: Illustrative Appearance - precedent images

7.0 PROPOSED DEVELOPMENT

7.4 INDICATIVE SCALE & APPEARANCE

Site 4

Crescent Residential Apartments

- The building steps from 4 storeys adjacent to the 3 storey offices in Thames Mews, to 5 and 6 storeys adjacent to Station Road.
- This block of apartments is proposed to be flat roof form with semi-recessed balconies. Facing the racecourse it is envisaged the glazing will create a contemporary high quality development.
- The illustrative section shows the relationship of the proposed scale with the closest existing property on Portsmouth Road (Churston House). The proposed building here is circa 47m from Churston House.

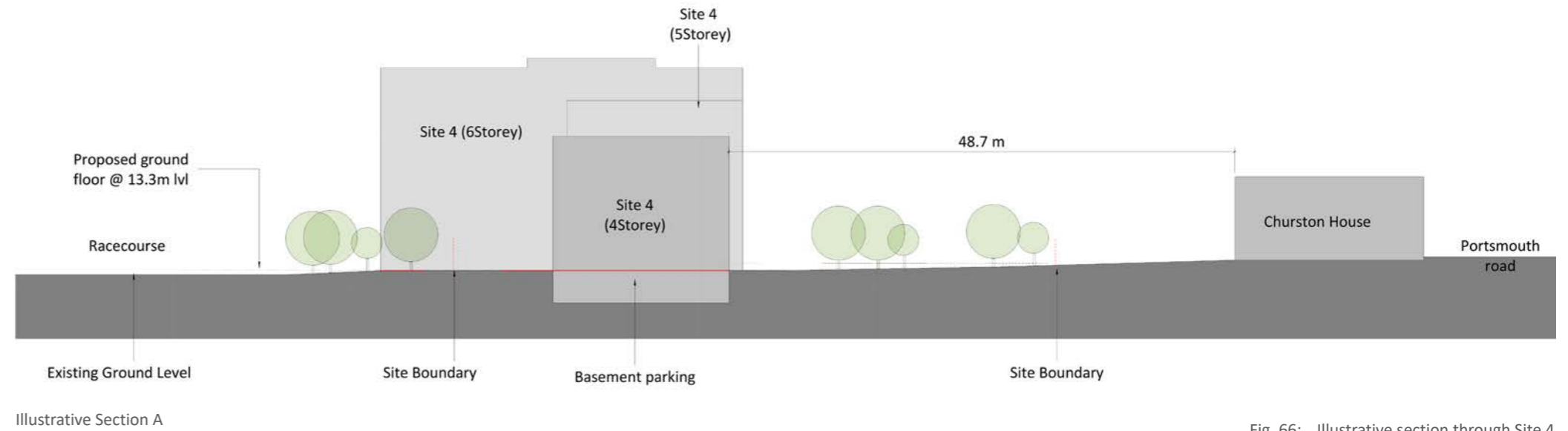


Fig. 66: Illustrative section through Site 4



Fig. 67: Illustrative Layout showing the buildings heights



Fig. 65: Illustrative Appearance - precedent images

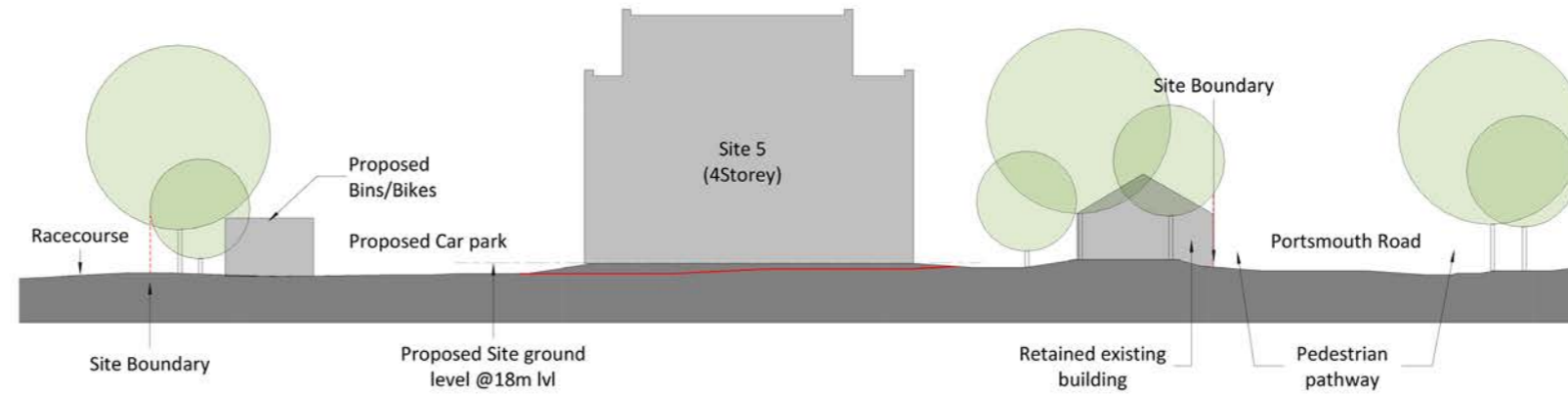
7.0 PROPOSED DEVELOPMENT

7.4 INDICATIVE SCALE & APPEARANCE

Site 5

Villas Residential Houses

- Each 'villa' is 4 storeys and currently proposed to be of flat roof form, however the design could accommodate pitched roofs which may help reduce the perceived building height by accommodating the top floor within the roof with dormer windows. The proposed fourth floor is set back from the main footprint and could be fully accommodated within a pitched roof.
- The design of the 'villas' would incorporate the design styles of the adjacent properties (Cheltonian Place and Wren House), extending this group of buildings in a coherent manner.
- The section shows the relationship of the proposed scale with the substantial tall tree and bush screening on both sides of the road.



Illustrative Section A

Fig. 68: Illustrative section through Site 5

Day Nursery

- The Nursery building has a similar footprint to the adjacent 'villas' but will step down to 2 storeys. This with the set-back from Portsmouth Road, will help with the transition in scale from the villas to open land (racecourse).

KEY	
	APPLICATION SITE BOUNDARY
	1 STOREY
	2 STOREYS
	3 STOREYS
	4 STOREYS

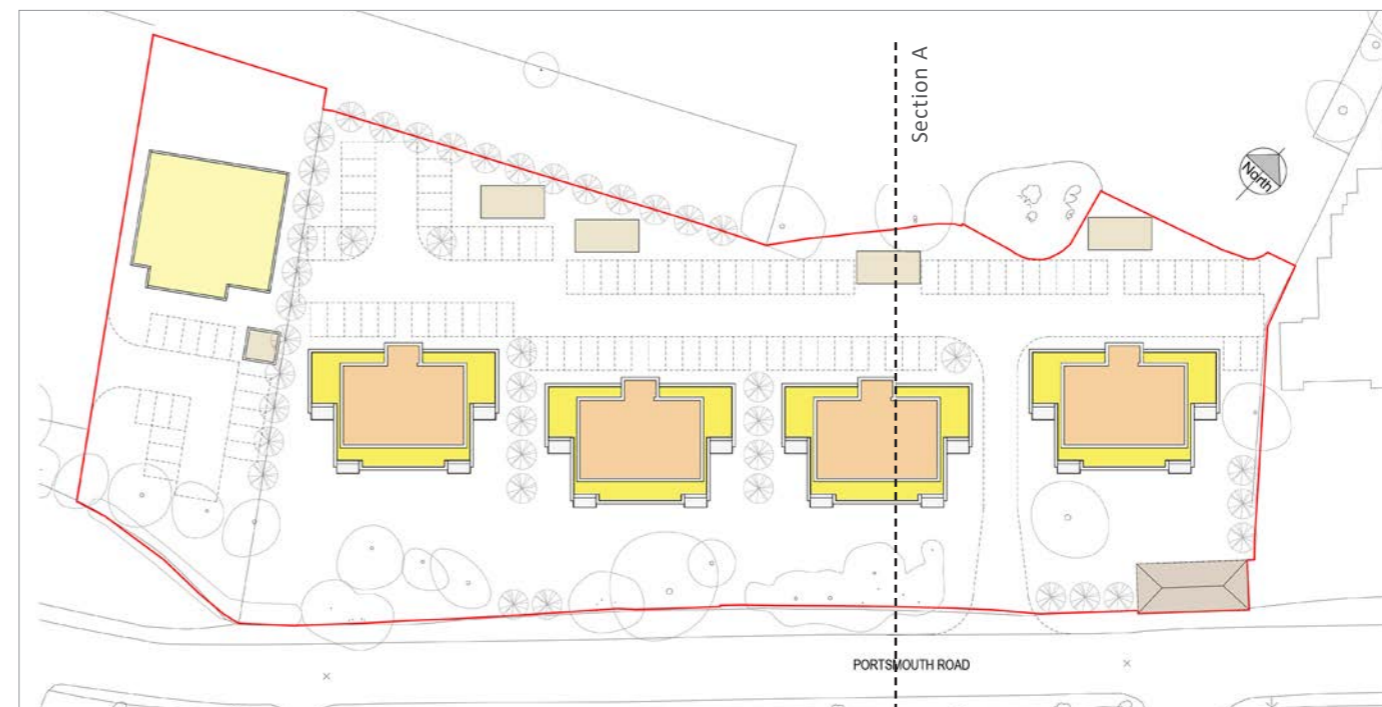


Fig. 69: Illustrative Layout showing the buildings heights

7.0 PROPOSED DEVELOPMENT

7.4 INDICATIVE SCALE & APPEARANCE

Site A

Racecourse Operations Area

- Site A is the main operations area for the horse and riders facilities.
- Predominantly all the structures within this area are single storey, the majority of which are the horse stables. The rider's staff 'hotel' and canteen will be 2 storeys.
- The new buildings will complement the existing buildings and preserved retaining walls from the back of the existing (demolished) stables. Stables and associated facilities will be mono-pitch roofs to retain the character of the existing stabling facilities.
- The staff 'hotel' will also have a pitched roof. Facing materials and design to be sympathetic to the character of the woodland backdrop of the Warren.

Site B

Hotel

- Site B is the new hotel. The proposal is 6 storeys with the upper 5 storeys narrower than the ground floor. The additional width to the ground floor creates an open viewing terrace at first floor.
- The overall height will be less than the roof to the adjacent Grandstand building.



Fig. 70: Site B Illustrative Appearance - precedent image. Doncaster Hotel

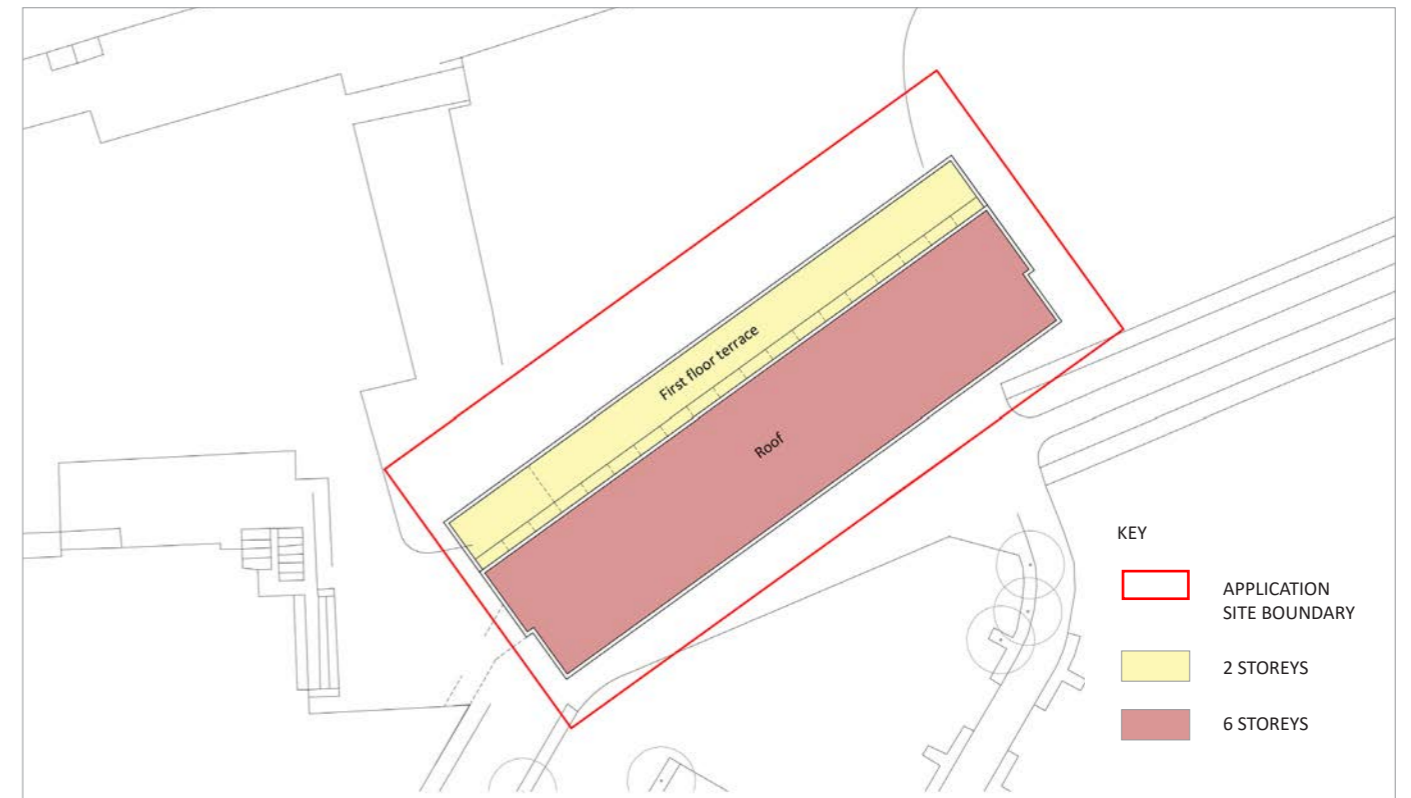


Fig. 71: Illustrative Site B Layout showing the buildings heights

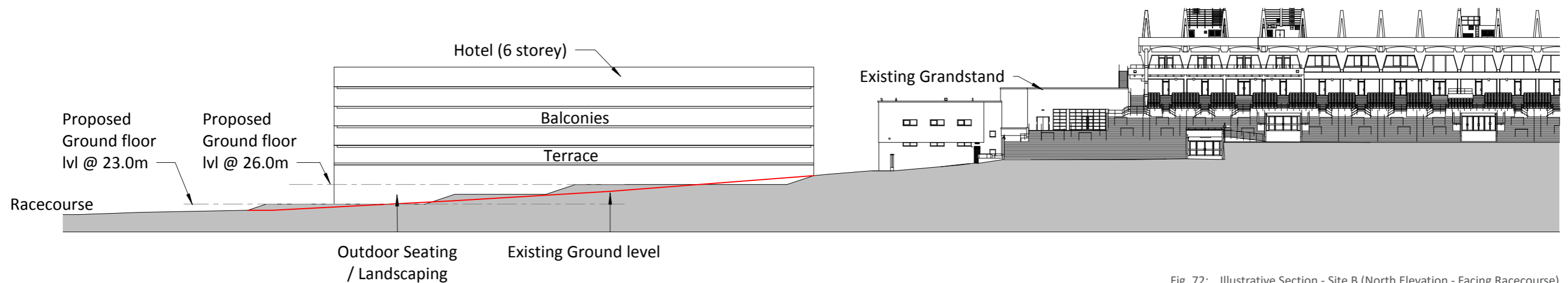


Fig. 72: Illustrative Section - Site B (North Elevation - Facing Racecourse)

7.0 PROPOSED DEVELOPMENT

7.4 INDICATIVE SCALE & APPEARANCE

Site C

Family / Community Zone

- Site C will provide new and revitalized community and family leisure facilities. Predominantly this will involve improvements and alterations to the landscaping, and removing existing buildings reducing the scale and visual impact of the site.
- The new café building will be single storey sensitively designed to suit the green belt, and reduce the visual impact. Natural materials such as timber cladding and large areas of glazing will help integration with the surrounding landscaping.
- The roof of the café will be visible from the Grandstand and should be designed to enhance the views across the park, including organic / curved roof, possibly with green roof cover.

Site D

Rationalised Car Parking Area

- Site D will provide optimised car parking. The aim is to retain the appearance of much of the green space, while making the area more suitable for car parking utilising a ground reinforcement system which would allow the grass to grow through.

Site F

Optimised Parking & Sandown Park Frontage

- New car / coach parking surfacing will be gravel bound asphalt or similar. The parking areas of grass reinforcement will retain the appearance of the green space.
- The new turnstile kiosk will be closely located to the Grandstand and will be constructed with facing materials in character with the Grandstand.
- The new broadcasting compound and Ring Main Unit will be screened with timber fencing and dense landscaping to reduce the visual impact on the site.

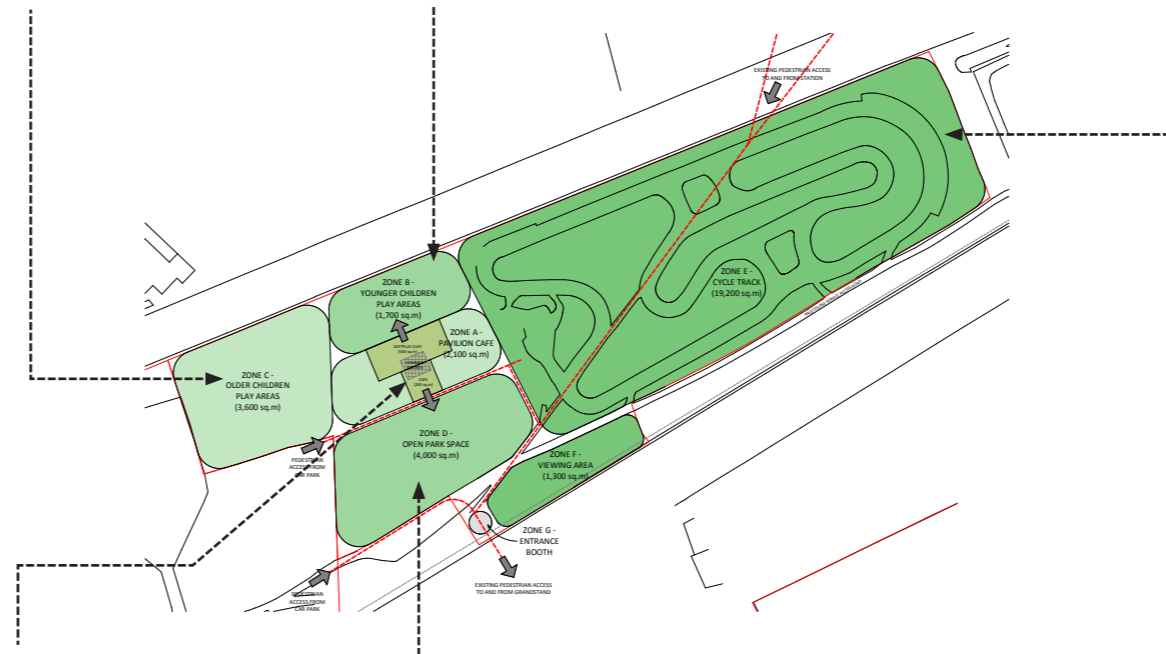
Zone C - Older Children Play Areas



Zone B - Younger Children Play Areas



Zone E - Cycle Track



Zone A - Pavilion Cafe



Zone D - Central Open Space

Fig. 73: Site C Illustrative Appearance - Precedent images

7.0 PROPOSED DEVELOPMENT

7.5 DETAILED ELEMENT

Sites E1 & E2

Racetrack Widening

- Sites E1 & E2 will provide additional width to the racetrack by relocating the guide rail and integrating the new track area with the existing track with the require specification of surfacing and drainage.
- Site E1 includes improvement to the existing road in from More Lane, with an improved specification of surfacing as it crosses the track providing a suitable all weather racing surface.
- Site E2 will involve relocating a number of golf course features and recourse drainage routes.

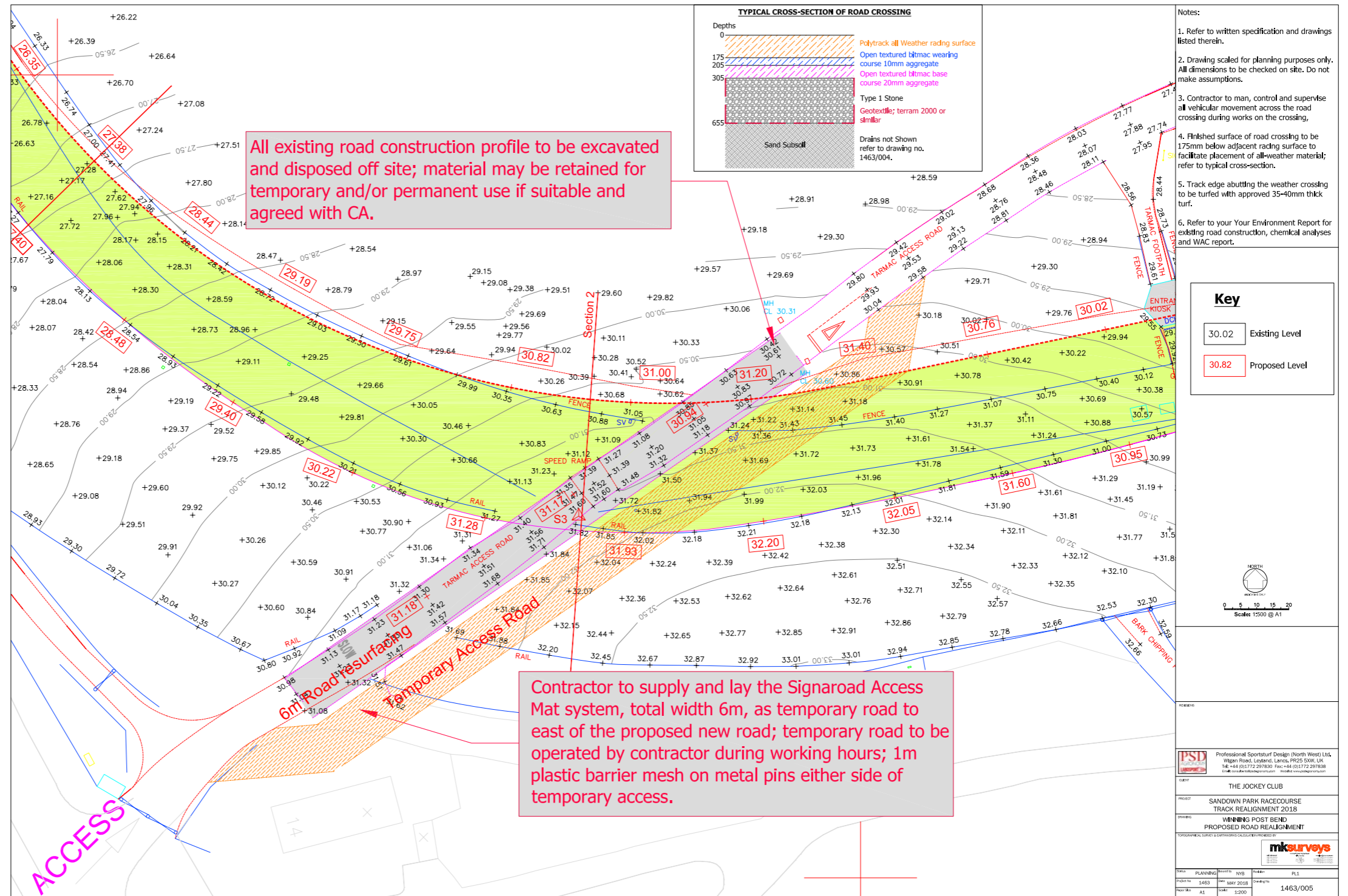


Fig. 74: Proposed Layout Site E1 - Refer to Application Drawing 1463 005 T1

7.0 PROPOSED DEVELOPMENT

7.5 DETAILED ELEMENT

Sites E1 & E2

Racetrack Widening

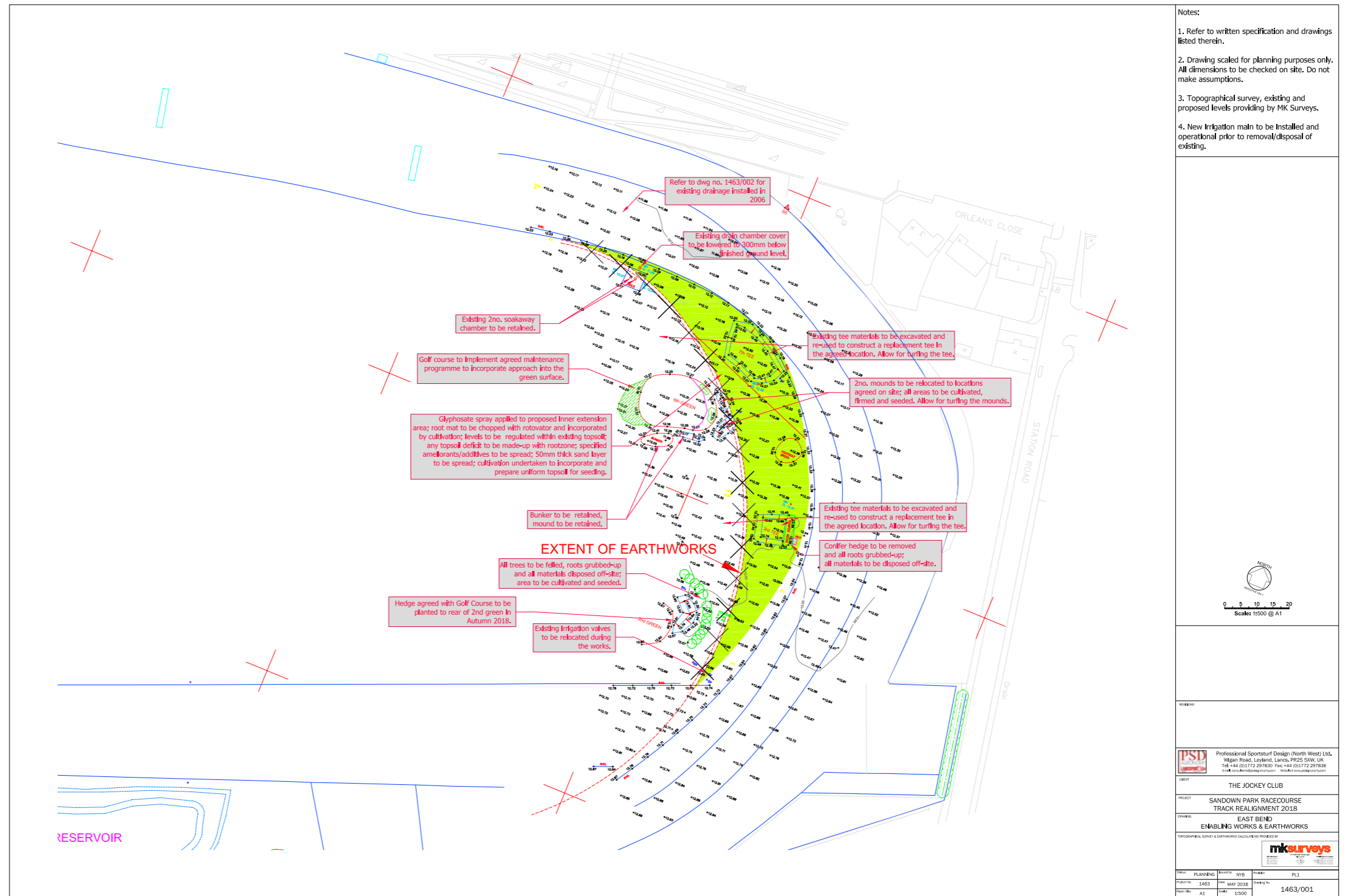


Fig. 75: Proposed Layout Site E2 - Refer to Application Drawing 1463 001 P3

7.0 PROPOSED DEVELOPMENT

7.6 ACCESS STATEMENT

The proposed development is an opportunity to address where the Racecourse's operations and facilities that have been identified by an Access Audit (prepared by Nick Bevan Consulting Ltd) are not currently meeting best practice standards. This includes:

- The installation of tactile paving/drop kerbs at crossing points.
- Pavement maintenance to address tree route damage.
- Providing clearly demarcated disabled car parking spaces.
- Resurfacing of current loose gravel car parking to provide additional, more safe and accessible disabled car parking spaces.
- Improving accessibility on approaches and at entrances as well as within W/Cs.
- Accessible staff accommodation to replace Sandown Park Lodge which does not currently have a disabled facility in any bedroom or lift to access upper floors.

Controlling Legislation and Design Guidance

The following legislation and design guidance has been taken into account, together with best practice determined through experience with a variety of clients and people with disabilities over a number of years.

Sources of Guidance

- Approved Document M (access to and use of buildings) of the Building Regulations
- Equality Act 2010, Part K 2013 edition (Protection from falling, collision and impact)
- British Standard 8300:2009 design of buildings and their approaches to meet the needs of disabled people - Code of practice.
- Guidance from other authoritative sources including the Centre for Accessible Environments, the RNIB, RNID etc.

Accessibility & Crime Reduction

Accessibility has been carefully considered as part of the design, and access will be provided for all and to all areas of the buildings and landscaping. Level access thresholds will be installed, and door widths meet or exceed minimum requirements under the Building Regulations. Lifts are provided to access all floors of the buildings.

Ground floor accommodation is separated from adjacent footpaths and vehicular areas with defensible space, landscaping or similar other measures. High quality secure windows and glazing to be provided at ground floor level.

The residential buildings will have access control systems fitted to main entrance doorways, allowing residents to manage those that enter the buildings. Good lighting will be provided to all accessible areas to the perimeter of the buildings.

Inclusive Access

Approach to Inclusive Design

It is intended to provide a development with safe, easy and inclusive access for all people regardless of disability, age or gender. The concept of access for all is based on an inclusive approach to design whereby all users and not just users with disabilities are taken into account. This access statement explains the design intention; it is acknowledged that the access provisions will require adjustment during the lifetime of the development.

This statement has been prepared based on guidance in BS8300:2009 Design of buildings and their approaches to meet the needs of disabled people - Code of practice and in Approved Document M 2015 (Access to and use of buildings) of the Building Regulations.

The new buildings will comply with the various requirements of the Equality Act 2010, Part K 2013 edition (Protection from falling, collision and impact) and Part M 2015 (Access to and use of buildings) of the Building Regulations.

The access philosophy and information particular to the building will be fully integrated into the long term building management, for example by handing on the access statement to the building operators to form the basis for ongoing monitoring and provision of accessible services as follows:

- Inclusive – so everyone can use it safely, easily and with dignity
- Responsive – taking account of what people say they need and want
- Flexible – so different people can use it in different ways
- Convenient – so everyone can use it without too much effort or separation
- Accommodating for all people, regardless of their age, gender, mobility, ethnicity or circumstances
- Welcoming – with no disabling barriers that might exclude some people
- Realistic – offering more than one solution to help balance everyone's needs and recognising that one solution may not work for all



7.0 PROPOSED DEVELOPMENT

7.6 ACCESS STATEMENT

Vehicular Access

Car parking is provided with easy access to the building entrances. Accessible car parking bays are provided as close to the entrances as possible, and with additional space to side and rear for manoeuvring into and out of the vehicle. Access to the buildings is assisted by provision of ramps, dropped kerbs and level access.

The apartment blocks have disabled car parking spaces in dedicated parking areas close to the entrance to the blocks. These bays will be defined with a contrasting painted demarcation colour or surfacing material, which extends to the side and front of each bay. Each bay also has a contrasting painted disabled logo in its centre.

Pedestrian & Cycle Access

Pedestrian access is enabled via dedicated footpaths from Portsmouth Road, More Lane, Lower Green Road and Station Road, with clear and logical routes to the main entrances, assisted by provision of ramps, dropped kerbs and level access into the building.

The footpaths from the site entrance will be generally a minimum of 1.5m wide. These footpaths should not have gradients steeper than 1:20.

Emergency Services Access

The access from Portsmouth Road, More Lane, Lower Green Road and Station Road, with the turning and manoeuvring space provided for large vehicles, allows emergency services good access to the main points of entry into all buildings.

Refuse Strategy

Dedicated refuse and recycling stores are located close to the building entrances to ensure easy access for residents and collection services.

Access into and from the Buildings Entrances will be accessible to disabled people and level with external surfacing. Disabled access to the buildings within the site will be either via road to the disabled car parking bays or via footpath from the site entrances.

Circulation in the Buildings

The objective will be to make reasonable provision for a disabled person to visit occupants who live on any storey. The lifts will be used in the buildings as an addition to common stairs as the most suitable means of access from the ground to the upper floors. The lifts will satisfy Approved Document M. The stairs will be designed to suit the needs of ambulant disabled people and people with impaired sight.

Access within the Apartments

The objective will be to facilitate access within the entrance storey or the principal storey of the building, into the habitable rooms, bathrooms and WC's. Door openings and corridor widths will be designed to accommodate wheelchair access at ground level.

Where required, wheelchair facilities will be provided in the units. Doors and WC provision will satisfy Approved Document M. The aim will be to assist those people whose reach is limited to use the building more easily by locating wall mounted switches and socket outlets at suitable heights. Accessible switches and sockets will satisfy Approved Document M.



7.0 PROPOSED DEVELOPMENT

7.7 INDICATIVE DELIVERY

The proposed developments are expected to be delivered in phases over several years.

Whilst the overall Phasing Plan remains subject to future approval, the provisional phasing plan is as follows:

Phase 1:

- Site 3

Phase 2:

- Sites 1 & 2
- Site A
- Sites E1 and E2
- Site D *
- Site C *

Phase 3:

- Site 5
- Site B
- Site F

Phase 4:

- Site 4

*Sites C & D phasing is subject to sales and funding and could be in Phase 3 or 4.

For further details please refer to the accompanying Outline Construction Environmental Management Plan.

8.0 CONCLUSIONS

8.1 DEVELOPMENT SUMMARY

This Design and Access Statement has demonstrated that this detailed application has been thoughtfully considered and has taken into account the varied requirements from both a design and planning policy perspective. The resulting proposals have been assessed against the following criteria:

Use

The proposed development is led by the proposed enhancements to the existing use of the site as a racecourse, supported by a discrete level of facilitating residential development within the Green Belt. As well as the racecourse operations and facilities, the site currently contains existing residential buildings. In this context, the proposed development accords with the site's established land uses. It also accords with the prevailing land use surrounding the site, which is predominately residential.

Amount

The site at the edge of the town centre and close to the local railway station, is an ideally placed location to access local amenities. The application delivers a realistic quantum of residential floorspace and hotel rooms without compromising planning policy or operational practicalities. The tenure of apartments is suitable for the market requirements in this location. The racecourse operational areas improve the existing facilities rather than increase capacity. The increase in recreational area and associated facilities is a benefit to the community, both locally and further afield.

Layout

The proposed indicative layouts are clear and legible and deliver a safe environment in a sustainable location, whilst responding to the site surroundings and helping to retain the openness of the Green Belt in a sympathetic manner. Buildings respond to their neighbours so as to retain privacy, and layouts take into account natural features and existing trees to minimise impact on the local environment

Scale

The proposed scale and design is considered appropriate for each site. The proposals in their scale maximise the development potential for the sites to provide much needed new homes, and in turn help maximise funding potential for the regeneration of Sandown Park.

Appearance

The treatment of buildings demonstrates that the applicant will deliver a high standard of product. Consideration for the local vernacular as well as a site led approach will give each site an identifying character and create a scheme which will be both visually distinctive yet sensitive to the adjacent buildings. It will positively enhance the character and appearance of the area.

Access

The development has been designed to be inclusive for all and gives occupiers the flexibility to adapt to any future changing requirements.

Accordingly, the proposed development is considered acceptable in design and access terms.

APPENDIX 1

DESIGN CODE

Site 1

- Access is from the existing access point on More Lane.
- Retain the boundary wall to the stables on the southern boundary.
- Ensure unrestricted access through from More Lane to the racecourse for emergency vehicles.
- Provide sufficient clearance between the building and neighbouring trees, both RPA's and overhanging canopies.
- Avoid north facing habitable rooms where possible, due to restricted daylighting from dense tree cover to the north.
- Provide sufficient privacy to properties to the south on More Lane. Minimise direct outlook from habitable rooms into closest properties 52 & 54. The existing height of the boundary wall ensures effective screening.
- Residential units will have amenity space of either a balcony or dedicated or communal ground floor garden area.
- Facing materials to be contextual with neighbouring properties (local brick, render) with attention paid to brick detailing and textural variation. Elements of timber would help respond to the neighbouring ancient woodland.
- The form could include pitched and / or flat roofs.
- Building heights to range from up to 2 storeys in the western end of the site to 3 storeys at the eastern end.
- Sandown Park Racecourse will remain closely involved in the design process with a subsequent developer of the site, in order to ensure that design is sympathetic to the Racecourse and aligns with Sandown Park's operational requirements.

Site 2

- Access is from Portsmouth Road, crossing over a section of privately owned land (Jockey Club Racecourses).
- The ground floor of the building will relate to the existing street levels at the southern corner of the site.
- Public step-free access will be provided across the site from the southern corner adjacent to No. 2 High Street through to the north eastern boundary.
- The building will set back from the south eastern boundary along Portsmouth Road a minimum of 7-m to allow sufficient space for public access, landscaping and private garden spaces.
- The existing trees within the south eastern boundary and associated banking will be removed to facilitate boundary improvements and street level integration.
- Provide sufficient privacy to adjacent properties on No.2 Warren Close, by minimising windows facing south west from habitable rooms and providing tree screening to the boundary, as necessary.
- Residential units will have amenity space of either a balcony or dedicated or communal ground floor garden area. Facing materials to be contextual with neighbouring properties and Esher High Street in general (local brick, render).
- The form could include pitched and / or flat roofs.
- Building heights to range from up to 3 storeys along Portsmouth Road (facing south east) to up to 4 storeys (facing north west and relative to Portsmouth Road level). The natural change of level from Esher High Street to the north east boundary is around 1 storey.
- The setting of Traveller's Rest will be enhanced by the improvements to the boundary of the Site 2/Portsmouth Road frontage.
- Sandown Park Racecourse will remain closely involved in the design process with a subsequent developer of the site, in order to ensure that design is sympathetic to the Racecourse and aligns with Sandown Park's operational requirements.

Site 3

- Access is from a new access point on Lower Green Road.
- The ground floor of the dwellings will take appropriate measures to comply with the Flood Risk Assessment for Site 3 and raised any habitable rooms above the projected levels defined in the FRA
- Remaining site levels will remain close to existing ground levels to avoid impacting on existing trees, where possible.
- Ensure unrestricted access through from Lower Green Road to the racecourse for emergency vehicles, and race day and event traffic management.
- Buildings will be located in the southern section of the site, to the south of the existing drain that runs west to east across the site.
- The northern section of the site (to the north of the existing drain) can accommodate limited car parking, sensitively located and constructed to avoid adverse impact to prominent existing trees. Generally this area will require additional landscaping improvements and tree management to facilitate a suitable amenity area, while providing screening to Lower Green Road.
- Residential units will have amenity space of either a balcony or dedicated or communal ground floor garden area. Facing materials to be contextual with neighbouring properties and More Lane in general (local brick, render).
- The development will create a new backdrop to the racecourse as viewed from the Grandstand and will require an appropriate design response to this setting.
- The form could include pitched and / or flat roofs.
- Building heights limited to 3 storeys.
- Sandown Park Racecourse will remain closely involved in the design process with a subsequent developer of the site, in order to ensure that design is sympathetic to the Racecourse and aligns with Sandown Park's operational requirements.

APPENDIX 1

DESIGN CODE

Site 4

- Access is from a new access point on Station Road.
- Residential units will have amenity space of either a balcony or dedicated or communal ground floor garden area. Facing materials to be contextual with neighbouring properties (local brick, render).
- The development will create a new backdrop as viewed from the racecourse and will be one of the first buildings seen on entering Esher from the station. This will require an appropriate design response to this setting.
- The form could include pitched and / or flat roofs.
- Building heights limited to 6 storeys.
- Sandown Park Racecourse will remain closely involved in the design process with a subsequent developer of the site, in order to ensure that design is sympathetic to the Racecourse and aligns with Sandown Park's operational requirements.

Site 5

- Access to the residential area is from a new access point from Portsmouth Road. Access to the new nursery area is retained from the existing access point from Portsmouth Road crossing over a section of privately owned land (Jockey Club Racecourses).
- There will be no access between the nursery area and the residential area.
- The original Toll House building will be retained with the later additions demolished. The retained Toll House will be renovated.
- Residential units will have amenity space of either a balcony or dedicated or communal ground floor garden area. Facing materials to be contextual with neighbouring properties (local brick, render).
- The buildings will set back from the south eastern boundary along Portsmouth Road a minimum of 14-15m to allow sufficient space for landscaping and private garden spaces, and retention of the prominent tree line along this boundary.
- The form could include pitched and / or flat roofs.
- Sandown Park Racecourse will remain closely involved in the design process with a subsequent developer of the site, in order to ensure that design is sympathetic to the Racecourse and aligns with Sandown Park's operational requirements.

Site A

- The specification and requirements for the operations area of Sandown Park Racecourse will be governed by the British Horse Racing Authority (BHA) guidelines.
- Access for horse box vehicles will be from the existing access point from Portsmouth Road at the eastern corner of the site. An emergency vehicle access will be retained through Site 1. Various service access routes run to and from the site from adjoining racecourse areas.
- The stables and ancillary facilities will single storey with mono-pitch roofs. The stable staff facilities (hostel, canteen and changing rooms) will sit at the northern boundary of the site:
 - » It will provide sufficient clearance between the building and neighbouring trees, both RPA's and overhanging canopies.
 - » Facing materials to be contextual with stables and ancillary facilities. Additionally, elements of timber would help respond to the neighbouring ancient woodland.
 - » The form could include pitched and / or flat roofs.
 - » Building height limited to 2 storeys.
- The horse box parking area will be configured for race day operations with construction suitable for heavy vehicles (up to 26 tonne). On non-race days the area will be used for additional car parking.
- The area adjacent to the access on Portsmouth Road will provide new landscaping with integrated car parking and public access into Sandown Park. The area will provide a new public access into the site from Esher High Street and will require an appropriate design response to this setting.

APPENDIX 1

DESIGN CODE

Site B

- Access to Site B will be from the existing access point from Portsmouth Road crossing over a section of privately owned land (Jockey Club Racecourses).
- The proposed hotel building will be a maximum of 6 storeys. The maximum height will not exceed the height of the upper concrete Grandstand canopy.
- The location of the hotel will consider the sight lines from the Grandstand seating to all areas of the racecourse.
- Facing materials to be contextual with neighbouring Grandstand building.
- The hotel should be considered as an extension to the Grandstand building with complementing styling, suitable of a building within a nationally prominent racecourse.
- Required parking provision will be integrated within Site F.

Site C

- Access to Site C will be from the existing access point on More Lane.
- The landscaping scheme will be clearly visible from the Grandstand and will require an appropriate design response to this setting.
- The café will be 1 storey and be integrated with its landscape setting.
- The café roofscape will be clearly visible from the Grandstand and will require careful consideration of materials.
- Plant and service areas to the café should not be visible from the Grandstand.
- The café form could include flat or curved roofs, incorporating living (green) roof cover.
- Facing materials to the café will include large areas of glazing. The main context is the Grandstand and racecourse, however there is an opportunity to design a more bespoke building using materials to complement a landscaped setting.

Site D

- Access to Site D will be from the existing access point on More Lane.
- New areas of parking using hardstanding will be a bound gravel type finish or similar.
- Areas of parking using grass reinforcement will provide a grass finish to match the adjacent grass car park areas.

End



THE JOCKEY CLUB